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HDR ENGINEERING  
120 BRENTWOOD COMMONS WAY, SUITE 525  
BRENTWOOD, TN 37027  
ANTHONY L. WASHINGTON, III, P.E. 119749

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.



Index Of Sheets  
SEE SHEET NO. 1A

PROJECT TO BE LET WITH:  
LAWRENCE, MAURY, LEWIS CO. SR-20  
PIN 131426.00

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
BUREAU OF ENGINEERING

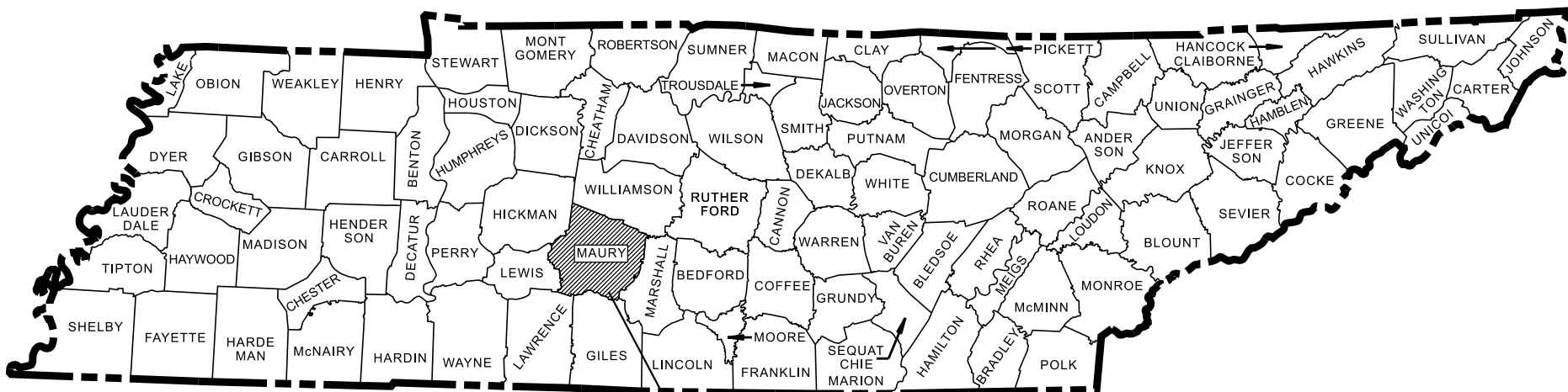
MAURY COUNTY

STATE ROUTE 6  
FROM: NEAR JAMES M. ROY ROAD (L.M. 0.90)  
TO: SOUTH OF FIRST AVENUE (L.M. 8.00)

RESURFACE  
TLD OVERLAY @ 85LB/SY, PAVEMENT MARKING & GUARDRAIL  
STATE HIGHWAY NO. 6 F.A.H.S. NO. 43

DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES X	NO

TENN.	YEAR	SHEET NO.
	2025	1
FED. AID PROJ. NO.		
STATE PROJ. NO.	60S006-S8-008	
STATE PROJ. NO.	60S006-S2-002	
STATE PROJ. NO.	60S006-M3-009	



PROJECT LOCATION  
BRIDGE ID. # 60SR0060084 60SR0060083 60SR0060085  
60SR0060009 60SR0060010 60SR0060031  
60SR0060032

NO EXCLUSIONS

60S006-S2-002  
END PROJECT NO. 60S006-S8-008 RESURFACE  
L.M. 8.00

BRIDGE DECK REPAIR PROJECT NO. 60S006-M3-009  
SR-6 @ L.M. 2.48 LT, L.M. 2.51 RT & L.M. 5.58 LT & RT

TSRR [350716T]  
PARALLEL OVERHEAD CROSSING # 60SR0060083  
TSRR M.P. A 0249.40  
L.M. 2.55

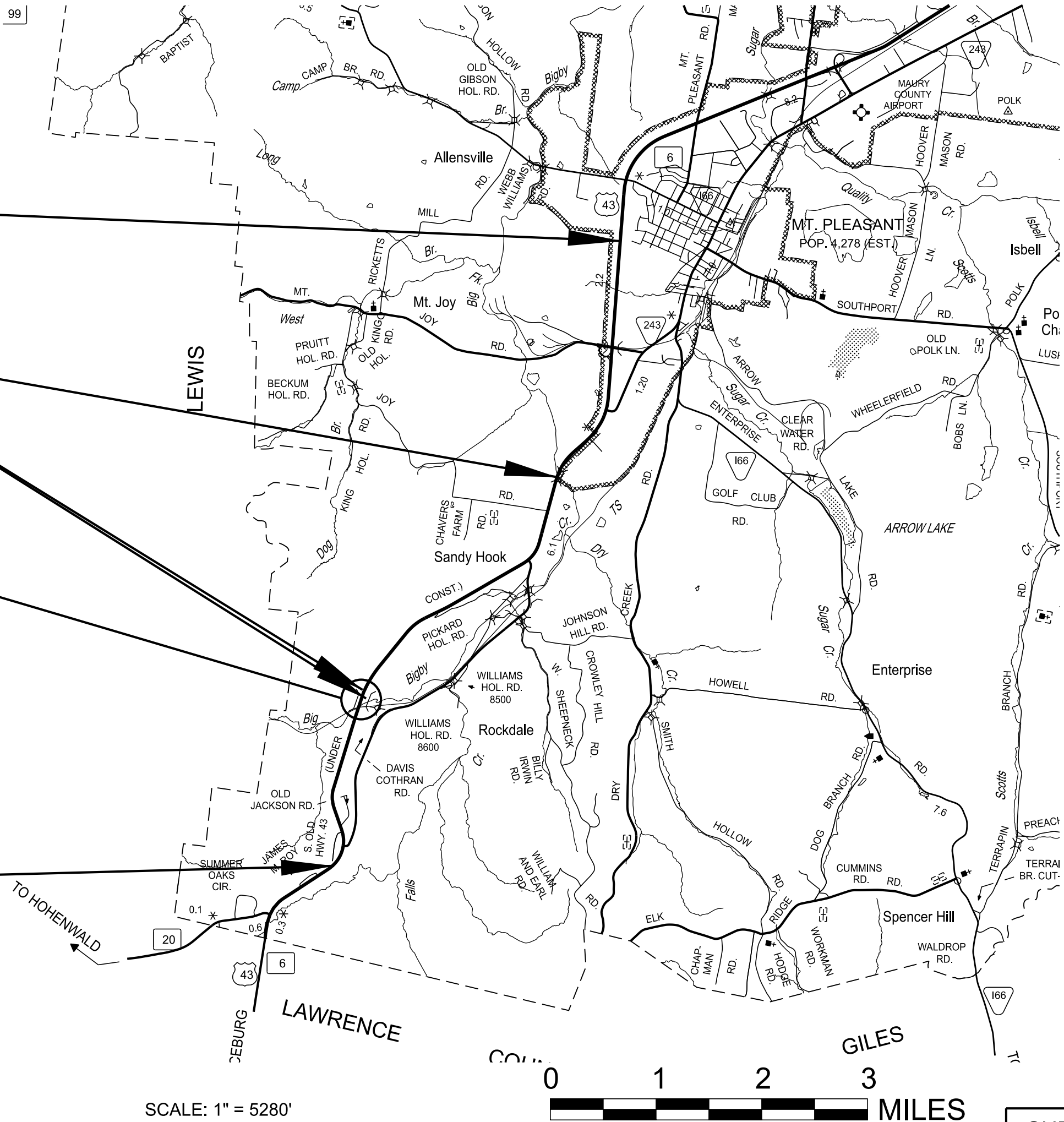
60S006-S2-002  
BEGIN PROJECT 60S006-S8-008 RESURFACE  
L.M. 0.90

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES  
CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW  
THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF  
THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND  
ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS  
AND IN THE PROPOSAL CONTRACT.

TDOT PROJECT MANAGER : KONNER SPRADLIN, P.E.  
DESIGNED BY : HDR ENGINEERING, INC.  
DESIGNER : ANTHONY L. WASHINGTON, III, P.E. CHECKED BY DAVID HORNE, P.E.  
P.E. NO. 98034-4299-04  
PIN NO. 122522.00

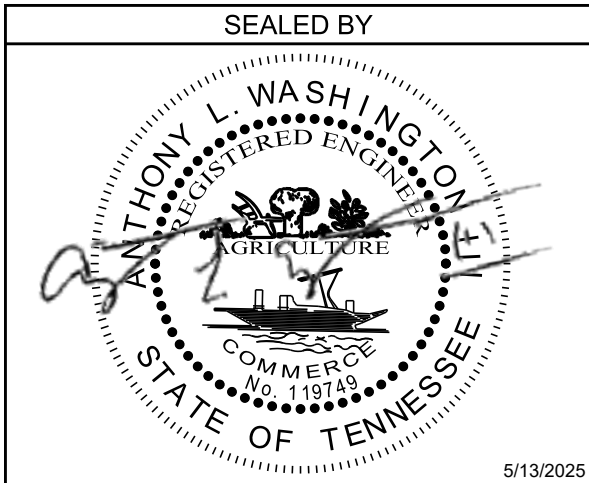


PROJECT LENGTH 7.10 MILES  
TOTAL LANE MILES RESURFACED 32.95 MILES

TRAFFIC COUNTER & WEATHER STATIONS	
STATION LOCATION	LOG MILE
TC STATION 94	5.513
TC STATION 178	6.533
TC STATION 115	7.263

SURVEY 8-12-21	TRAFFIC DATA	
	ADT (2025)	17,354
POSTED SPEED LIMITS		
	L.M. 0.90 - L.M. 1.30	@ 50 MPH
	L.M. 1.30 - L.M. 7.88	@ 65 MPH
	L.M. 7.88 - L.M. 8.00	@ 55 MPH

COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE  
FACTOR OF 1.00007 AND TIED TO THE TGRN. ALL ELEVATIONS  
ARE REFERENCED TO THE NAVD 1988 WITH GEOID 12B.



APPROVED: WILL REID, DEPUTY COMMISSIONER/  
CHIEF ENGINEER

DATE:

APPROVED: HOWARD H. ELEY, DEPUTY GOVERNOR &  
COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION  
FEDERAL HIGHWAY ADMINISTRATION  
APPROVED: DIVISION ADMINISTRATOR DATE

ROADWAY INDEX

SHEET NAME	SHEET NO.
SIGNATURE SHEET .....	ROADWAY-SIGN1
SIGNATURE SHEET .....	ROADWAY-SIGN2
TITLE SHEET .....	1
ROADWAY INDEX, STANDARD ROADWAY DRAWINGS AND STANDARD	
TRAFFIC DESIGN DRAWINGS .....	1A
PROJECT COMMITMENTS .....	1B
ESTIMATED ROADWAY QUANTITIES .....	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE .....	2A
GENERAL NOTES .....	2B
SPECIAL NOTES .....	2C
ENVIRONMENTAL NOTES .....	2D, 2D1
TABULATED QUANTITIES .....	2E
PAVEMENT MARKING IMPROVEMENTS .....	2F, 2F1 – 2F5
RIGHT-OF-WAY NOTES, UTILITY NOTES AND UTILITY OWNERS .....	3
PROPERTY MAP AND RIGHT-OF-WAY ACQUISITION TABLE .....	3A
PRESENT LAYOUT .....	4
RIGHT OF WAY DETAILS .....	4A, 4A1
BRIDGE PLANS .....	B1

NOTE: THE ALPHABETICAL LETTERS “I”, “O”, & “Q” ARE NOT USED IN THE  
NUMBERING OF SHEETS

NO UTILITY SHEETS INCLUDED IN THIS SET OF PLANS

STANDARD ROADWAY DRAWINGS

DWG.	REV.	DESCRIPTION
STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS		
RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	02-20-20	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND
SAFETY DESIGN AND GUARDRAILS		
S-GR31-1	06-15-21	GUARDRAIL DETAILS
S-GR31-1A	06-28-19	GUARDRAIL AND BLOCK-OUT DETAILS
S-GR31-1B		GUARDRAIL FASTENING HARDWARE
S-GR31-1C	07-07-23	GUARDRAIL GENERAL NOTES AND POST DETAILS
S-GR31-1D	03-01-23	GUARDRAIL POST PLACEMENT IN ROCK
S-GRT-2	06-28-19	TYPE 38 GUARDRAIL END TERMINAL
S-GRT-2R	06-28-19	EARTH PAD FOR TYPE 38 AND TYPE 21 TERMINAL (RETROFIT)

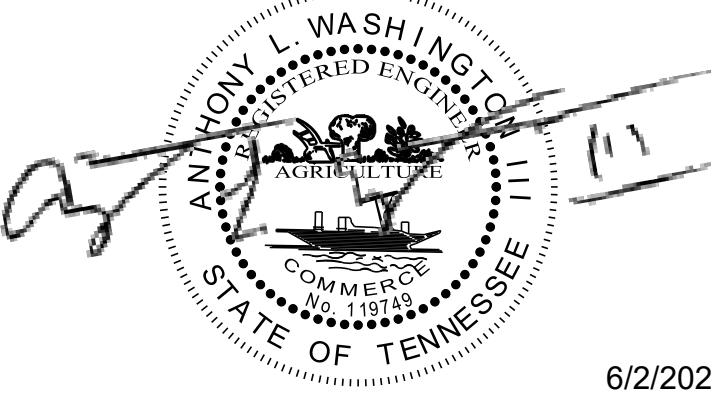
STANDARD TRAFFIC DESIGN DRAWINGS

DWG.	REV.	DESCRIPTION
DESIGN - TRAFFIC CONTROL		
T-M-1	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-M-3	01-24-25	MARKING STANDARDS FOR TRAFFIC ISLANDS, PAVED SHOULDERS AND MEDIANS FOR CONVENTIONAL ROADS
T-M-4	01-24-25	STANDARD INTERSECTION PAVEMENT MARKINGS
T-M-15A	01-24-25	ASPHALT SHOULDER RUMBLE STRIP INSTALLATION DETAILS FOR NON-ACCESS CONTROLLED RURAL ROUTES
T-M-18	01-24-25	FLEXIBLE DELINEATOR DETAILS
T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-11	03-04-21	ONE LANE CLOSURE DETAIL ON DIVIDED HIGHWAYS
T-WZ-12	03-04-21	ONE LANE CLOSURE DETAIL FOR BRIDGES ON DIVIDED HIGHWAYS
T-WZ-60		FREEWAY RESURFACING SIGNING LAYOUT
T-WZ-63	01-09-24	WORK ZONE IN THE VICINITY OF AN ENTRANCE RAMP
T-WZ-64		WORK ZONE IN THE VICINITY OF AN EXIT
T-WZ-65		LANE CLOSURE WITH LATE MERGE
T-WZ-FAB1		FLASHING YELLOW ARROW BOARD

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	60S006-S8-008	1A

REV. 06-02-25: ADDED ROADWAY-SIGN 2 TO INDEX.

SEALED BY



6/2/2025

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

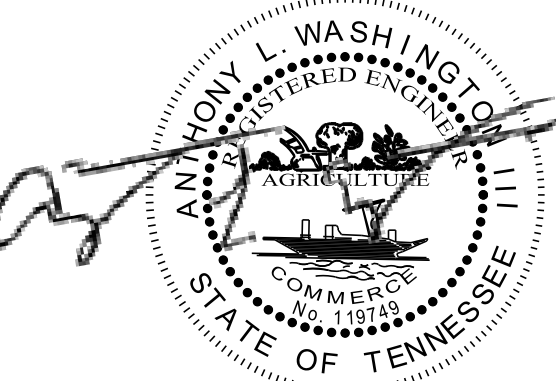
ROADWAY INDEX,  
STANDARD ROADWAY  
DRAWINGS AND  
STANDARD TRAFFIC  
DESIGN DRAWINGS



PROJECT COMMITMENTS			
COMMITMENT ID	SOURCE DIVISON	DESCRIPTION	STATION / LOCATION
EDHZ001	ENVIRONMENTAL DIVISION, HAZARDOUS MATERIALS	ASBESTOS CONTAINING MATERIAL (ACM) SURVEYS WERE COMPLETED ON BRIDGE NO. 60SR0060083 SR-6 NB OVER BIG BIGBY CREEK / TSRR LM 2.51 (60-SR006-02.51) AND BRIDGE NO. 60SR0060084 SR-6 SB OVER BIG BIGBY CREEK / TSRR LM 2.48 (60-SR006-02.48). NO ACM WAS DETECTED. PLEASE SEE THE REPORTS FOR FURTHER DETAILS AND PHOTOGRAPHS. NO SPECIAL ACCOMMODATIONS FOR DEMOLITION AND WASTE DISPOSAL ARE ANTICIPATED FOR THESE STRUCTURES AND THE MATERIAL CAN BE DEPOSITED IN A C&D LANDFILL. PRIOR TO THE DEMOLITION OR REHABILITATION OF ANY STRUCTURE (BRIDGE OR BUILDING), THE CONTRACTOR IS REQUIRED TO SUBMIT THE NATIONAL EMISSION STANDARDS FOR HAZARDOUS AIR POLLUTANTS STANDARD 10-DAY NOTICE OF DEMOLITION TO THE TDEC DIVISION OF AIR POLLUTION CONTROL (PER TDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (JANUARY 1, 2021) SECTIONS 107.08.D AND 202.03). THESE SURVEYS WERE COMPLETED UNDER PIN 129501.00, BUT APPLY TO THE LOG MILES IN THIS PROJECT.	BRIDGE NO. 60SR0060083 SR-6 NB OVER BIG BIGBY CREEK/ TSRR LM 2.51 (60-SR0106-02.51) AND BRIDGE NO. 60SR0060084 SR-6 SB OVER BIG BIGBY CREEK/ TSRR LM 2.48 (60-SR006-02.48)

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	60S006-S8-008	1B

SEALED BY



5/13/2025

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

PROJECT  
COMMITMENTS

ESTIMATED ROADWAY QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY 60S006-S8-008
(1)(2) 208-01.05	Brooming & Degrassing Shoulders	L.M.	15.5
(2) 303-01	Mineral Aggregate, Type A Base, Grading D	TON	3758
(3) 307-01.10	Asphalt Concrete Mix (Pg64-22) (Bpmb-Hm) Grading C-W	TON	200
(4) 403-01	Bituminous Material For Tack Coat (Tc)	TON	161
411-03.34	Acs Mix(Pg64-22) Thin Lift E Shoulder	TON	3572
(5) 411-03.13	Acs Mix(Pg70-22) Thin Lift D Asphalt	TON	10199
411-12.01	Scoring Shoulders (Continuous) (16In Width)	L.M.	21.7
(6) 415-01.01	Cold Planing Bituminous Pavement	TON	454
(7) 705-04.09	Earth Pad For Type 38 Gr End Treatment	EACH	1
(7) 705-06.20	Tangent Energy Absorbing Term Mash TI-3	EACH	1
(7) 706-01	Guardrail Removed	L.F.	50
(8) 712-01	Traffic Control	LS	0.6
712-04.01	Flexible Drums (Channelizing)	EACH	75
(9) 712-06	Signs (Construction)	S.F.	2675
712-08.03	Arrow Board (Type C)	EACH	2
713-02.14	Flexible Delineator (White)	EACH	15
(10) 713-15.01	Removal Of Existing Sign Post	EACH	3
713-16.01	Changeable Message Sign Unit	EACH	2
(10) 713-16.23	Signs (R5-1A)	EACH	15
(10) 713-16.24	Signs (R6-1L)	EACH	13
(10) 713-16.25	Signs (R6-1R)	EACH	11
(10) 713-16.26	Signs (R6-3)	EACH	3
(10) 713-16.27	Signs (R1-2)	EACH	10
(10) 713-16.28	Signs (R5-1)	EACH	16
716-01.22	Snowplowable Raised Pavment Markers (Mono-Dir)(1 Color)	EACH	610
716-01.23	Snowplowable Raised Pavement Markers (Bi-Dir)(2 Color)	EACH	937
716-01.30	Removal Of Snowplowable Reflective Marker	EACH	1547
(10)(11)(12) 716-02.04	Plastic Pavement Marking(Channelization Striping)	S.Y.	332
(11)(12)(13) 716-02.05	Plastic Pavement Marking (Stop Line)	L.F.	79
(10)(11)(12) 716-02.06	Plastic Pavement Marking (Turn Lane Arrow)	EACH	5
(10)(12) 716-02.08	Plastic Pavement Marking (8" Dotted Line)	L.F.	306
(10)(12) 716-02.11	Plastic Pavement Marking (6" Dotted Line)	L.F.	97
(12) 716-02.12	Plastic Pavement Marking (8In Line)	L.M.	0.2
(10)(12) 716-04.12	Plastic Pavement Marking (Yield Line)	S.F.	27
(15) 716-09.57	Contrast Pvmt Shadow Marking 6" (Tape)	L.F.	942
(12) 716-12.02	Enhanced Flatline Thermo Pvmt Mrkng (6In Line)	L.M.	32.1
(10)(12) 716-12.05	Enhanced Flatline Thermo Pvmt Mrkng (6In Dotted Line)	L.F.	4363
(12)(14) 716-15.10	Preformed Permanent Tape (6In Line)	L.F.	7548
717-01	Mobilization	LS	0.6

THERE ARE NO UTILITY ADJUSTMENTS ON THIS PROJECT

FOOTNOTES	
(1)	INCLUDES THE COST OF REMOVING DEBRIS AND SWEEPING SHOULDERS PRIOR TO WORK. SEE SHEET 2C, PAVEMENT, RESURFACING, NOTE 2 FOR MORE INFORMATION.
(2)	TO BE USED AS DIRECTED BY ENGINEER.
(3)	TO BE USED FOR SPOT REPAIR.
(4)	INCLUDES 1 TONS FOR PRIVATE DRIVES, COUNTY ROADS, FIELD ENTRANCES, INTERSECTIONS, CROSS OVERS AND TURN LANES.
(5)	TO BE USED FOR TRANSITION AREAS AT PROJECT LIMITS, L.M. 0.90 AND L.M. 8.00 L.M., AT BRIDGE TRANSITION AREAS, L.M. 2.48 NB AND L.M. 2.51 NB, AND AT EXTRA WIDTH AREAS FROM L.M 6.95 TO L.M. 7.02 NB, L.M. 7.61 TO 7.65 NB, L.M. 6.94 TO L.M 7.16 SB, AND AS L.M. 6.08 SB.
(6)	TO BE USED FOR TRANSITION AREAS AT PROJECT LIMITS, L.M. 0.90 AND L.M. 8.00 L.M.
(7)	SEE GUARDRAIL TABULATION ON SHEET 2E.
(8)	THE CONTRACTOR SHALL COMPLY WITH SECTION 712 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION REGARDING TEMPORARY TRAFFIC CONTROL AND THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
(9)	THE CONTRACTOR IS RESPONSIBLE FOR THE STAKING OF CONSTRUCTION SIGNS. IN THE EVENT THAT A CONSTRUCTION AND/OR REGULATORY SIGN IS TEMPORARILY DESIGNATED NOT IN USE DURING THE CONSTRUCTION PHASE OF A PROJECT, THE CONTRACTOR SHALL CHOOSE A SIGN COVERING APPROVED BY THE ENGINEER. TEMPORARY SIGN COVERINGS SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 712-06 SIGNS (CONSTRUCTION)
(10)	SEE PAVEMENT MARKING SHEETS 2F, 2F1 - 2F5. USE AS DIRECTED BY ENGINEER.
(11)	THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
(12)	ITEM TO BE USED AS PERMANENT MARKING ONLY
(13)	ALL STOP BARS ON SIDE STREETS WILL BE RETRACED AS DIRECTED BY THE ENGINEER.
(14)	FOR PERMANENT EDGE LINE AND CENTERLINE MARKING ON CONCRETE SURFACES FOR BRIDGES AT L.M. 2.48 AND L.M. 2.51.
(15)	FOR PERMANENT LANE LINE MARKING ON CONCRETE SURFACES. ITEM COST INCLUDES 471 L.F. WHITE MARKING AND 471 L.F. BLACK MARKING PAID IN PLACE FOR BRIDGES AT L.M. 2.48 AND L.M. 2.51.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	60S006-S8-008	2

REV. 06-02-25: REMOVED PAY ITEMS 713-16.21, 713-16.22, 716-08.30 AND 716-09.94. ADDED PAY ITEMS 713-16.27, 713-16.28 AND 716-09.57.

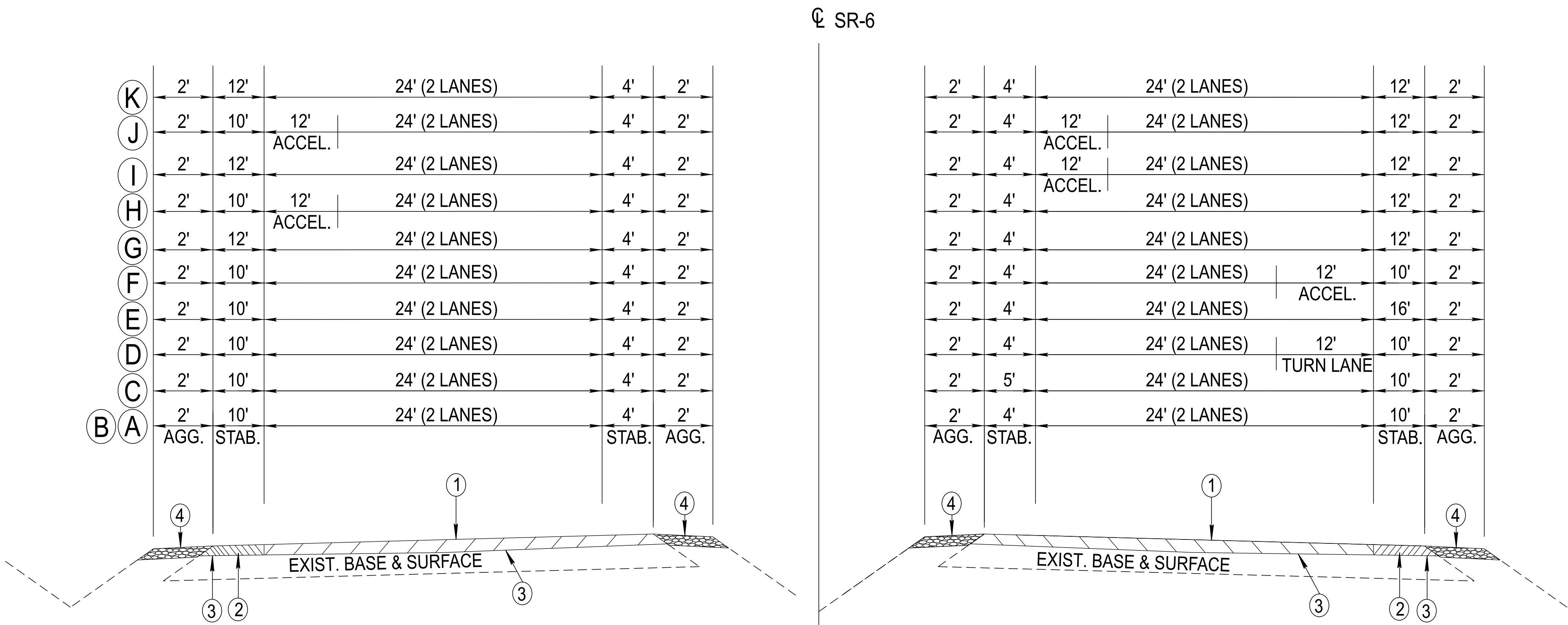
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6/2/2025

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

ESTIMATED  
ROADWAY  
QUANTITIES

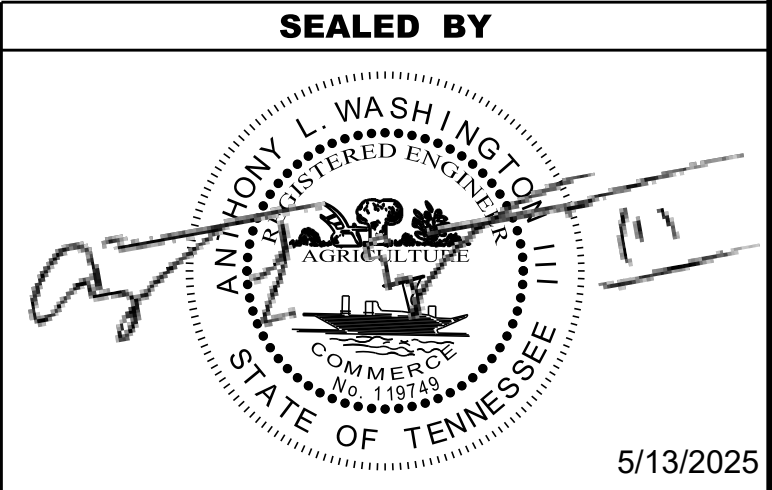


- STATE ROUTE 6
- (A) FROM: L.M. 0.90 TO L.M. 2.48
  - (B) FROM: L.M. 2.48 TO L.M. 6.07
  - (C) FROM: L.M. 6.07 TO L.M. 6.30
  - (D) FROM: L.M. 6.30 TO L.M. 6.36
  - (E) FROM: L.M. 6.36 TO L.M. 6.42
  - (F) FROM: L.M. 6.42 TO L.M. 6.56
  - (G) FROM: L.M. 6.56 TO L.M. 6.74
  - (H) FROM: L.M. 6.74 TO L.M. 6.87
  - (I) FROM: L.M. 6.87 TO L.M. 6.93
  - (J) FROM: L.M. 6.93 TO L.M. 7.05
  - (K) FROM: L.M. 7.05 TO L.M. 7.96 SB/ L.M 8.05 NB

PROPOSED PAVEMENT SCHEDULE	
①	ASPHALTIC CONCRETE SURFACE (ACS) @ 0.80"± THICK (APPROX. 85.00 LBS./S.Y.) ITEM NO. 411-03.12 ACS MIX (PG64-22) THIN LIFT D ASPHALT
②	ASPHALTIC CONCRETE SURFACE (ACS) @ 0.80"± THICK (APPROX. 82.40 LBS./S.Y.) ITEM NO. 411-03.34 ACS MIX (PG64-22) THIN LIFT E SHOULDER
③	TACK COAT (TC) SEE 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD ITEM NO. 403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC)
④	MINERAL AGGREGATE BASE (SHOULDERS) 2" TH. ITEM NO. 303-01 MINERAL AGGREGATE, TYPE A BASE, GRADING D

BRIDGE DECK RECOMMENDATIONS (RESURFACING)				
BRIDGE NUMBER	LOCATION LOG MILE	CROSSES OVER/UNDER	BRIDGE LENGTH	BRIDGE DECK RECOMMENDATIONS
60SR0060084	2.480	BIG BIGBY CREEK/ SB LNS. ONLY & TSRR	985'	SEAL WITH TYPE 1 THIN EPOXY OVERLAY/ REPLACE BRIDGE JOINTS (BRIDGE SHEETS INCLUDED)
60SR0060083	2.510	BIG BIGBY CREEK/ NB LNS. ONLY & TSRR	900'	SEAL WITH TYPE 1 THIN EPOXY OVERLAY/ REPLACE BRIDGE JOINTS (BRIDGE SHEETS INCLUDED)
60SR0060085	5.350	BRANCH	21'	PAVE WITH PLANS MIX/ TREATMENT TYPE
60SR0060009	5.580	BIG BIGBY CREEK	309.5'	COLD PLANE 1.25" OF THE EXISTING ASPHALT AND REPLACE WITH 1.20" OF NEW ASPHALT/ REPLACE BRIDGE JOINTS (BRIDGE SHEETS INCLUDED)
60SR0060010	5.580	BIG BIGBY CREEK	309.5'	COLD PLANE 1.25" OF THE EXISTING ASPHALT AND REPLACE WITH 1.20" OF NEW ASPHALT/ REPLACE BRIDGE JOINTS (BRIDGE SHEETS INCLUDED)
60SR0060031	6.960	MT JOY RD. RT & LT AND SSRR	209'	COLD PLANE 1.25" OF THE EXISTING ASPHALT AND REPLACE WITH 1.20" OF NEW ASPHALT
60SR0060032	6.960	MT JOY RD. RT & LT AND SSRR	209'	COLD PLANE 1.25" OF THE EXISTING ASPHALT AND REPLACE WITH 1.20" OF NEW ASPHALT

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	60S006-S8-008	2A



STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS AND PAVEMENT SCHEDULE



GENERAL NOTES

GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (3) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

GUARDRAIL

- (2) THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.
- (3) IF ANY APPROACH END OF A SECTION OF GUARDRAIL OR BRIDGE RAIL MUST TEMPORARILY BE LEFT INCOMPLETE AND EXPOSED TO TRAFFIC, THE CONTRACTOR SHALL USE TWO (2) TEMPORARY BARRICADES OR DRUMS WITH TYPE “A” LIGHTS AND ROUNDED END ELEMENTS AS MINIMUM MEASURES TO PROTECT TRAFFIC FROM THE HAZARD OF AN EXPOSED END. ALL COST OF FURNISHING AND INSTALLING TEMPORARY BARRICADES OR DRUMS WITH TYPE “A” LIGHTS TO DELINEATE GUARDRAIL END AND A TEMPORARY ROUNDED END ELEMENT SHALL BE INCLUDED IN THE COST OF THE PROPOSED GUARDRAIL END TERMINAL.

MISCELLANEOUS

- (2) THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES AND POSTS WHERE AND AS DIRECTED BY THE ENGINEER. COST TO BE INCLUDED IN PRICE BID FOR OTHER CONSTRUCTION ITEMS.
- (3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

FINAL PAVEMENT MARKING

- (6) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:

a. BROOMING & DE-GRASSING SHOULDERS SHALL INCLUDE CLIPPING OF MATERIAL INTERFERING WITH PROPER DRAINAGE OF ROADWAY (INCLUDING PAVED AND GRAVEL SHOULDERS), AS DIRECTED BY THE TDOT PROJECT ENGINEER.

b. ALL MATERIAL FROM CLIPPING, BROOMING AND DE-GRASSING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED AS DIRECTED BY THE TDOT PROJECT ENGINEER.

c. ALL COSTS ASSOCIATED WITH PICKING UP, REMOVAL AND PROPER DISPOSAL SHALL BE PAID FOR UNDER ITEM NO. 208-01.05.

d. REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (8) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6” ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

SNOWPLOWABLE REFLECTIVE PAVEMENT MARKERS

- (19) REMOVE EXISTING SNOWPLOWABLE MARKERS PRIOR TO PAVING AND/OR COLD PLANING. REMOVE ALL ADHESIVES PRIOR TO PAVING. PATCH ANY HOLES OR DIVOTS RESULTING FROM THE REMOVAL OF A MARKER IN A MANNER WHICH ENSURES A UNIFORM PAVED SURFACE. PATCH WORK SHALL BE INCLUDED WITH COST OF OTHER ITEMS OF CONSTRUCTION.

PAVEMENT

PAVING

- (2) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

- (4) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (5) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.
- (6) PRIVATE DRIVEWAYS, FIELD ENTRANCES, AND BUSINESS ENTRANCES WILL BE RESURFACED A PAVER WIDTH (LANE WIDTH) AS A MINIMUM. A PAVEMENT TAPER TO TRANSITION THE NEW PAVEMENT SHALL BE REQUIRED, IT SHALL BE BASED ON AN ADDITIONAL ONE FOOT OF WIDTH PER ONE INCH DEPTH OF PAVEMENT. IF THE SHOULDER IS NARROW ENOUGH THAT THE SUM OF THE SHOULDER AND THE TRANSITION ARE LESS THAN A PAVER WIDTH, THE TRANSITION SHALL OCCUR WITHIN THE PAVER WIDTH. IF THE SUM OF THE SHOULDER AND THE TRANSITION IS GREATER THAN A PAVER WIDTH (LANE WIDTH), THE TRANSITION SHALL OCCUR OUTSIDE OF THE PAVER WIDTH.
- (9) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

SIGNING

- (12) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.


CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.

- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER’S APPROVAL TO USE THEM.
- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER’S APPROVAL TO USE THEM.
- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (9) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

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SPECIAL NOTES

MISCELLANEOUS

- (1) THE CONTRACTOR SHALL TAKE EXTREME CARE WHEN WORKING AROUND EXISTING GUARDRAIL, HIGHWAY SIGNS, OR ANY DEPARTMENT OF TRANSPORTATION STRUCTURE TO AVOID DAMAGE DURING THE COURSE OF CONSTRUCTION. ANY DAMAGE WILL BE REPLACED OR REPAIRED AT HIS OWN EXPENSE.
- (2) ALL SIGNING AND PAVEMENT MARKINGS ARE TO BE IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIGORM TRAFFIC CONTROL DEVICES

PAVEMENT MARKINGS

- (2) THE CONTRACTOR IS TO PROVIDE THE LAYOUT OF ALL EXISTING PAVEMENT MARKINGS BEFORE BEGINNING THE RESURFACING PROJECT. EXISTING MARKINGS SHALL BE REFERENCED AS NEEDED TO ENSURE THAT ALL NEW MARKINGS ARE REPLACED IN THEIR CORRESPONDING LOCATIONS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

PAVEMENT

PAVING

- (4) SURFACE TO BE CROWNED AS DIRECTED BY THE ENGINEER.

RESURFACING

- (2) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:

A. BROOMING & DEGRASSING SHALL INCLUDE NOTCHING THE GRAVEL SHOULDER PRIOR TO MILLING. THE NOTCH SHALL BE 1.5 IN DEEP AND 2 FT WIDE, OR AS DIRECTED BY THE TDOT PROJECT ENGINEER.

B. ALL MATERIAL FROM NOTCHING AND BROOMING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED AS DIRECTED BY THE TDOT PROJECT ENGINEER.

C. ALL COSTS ASSOCIATED WITH NOTCHING, PICKING UP, REMOVAL AND PROPER DISPOSAL SHALL BE PAID FOR UNDER ITEM NO. 208-01.05.
- (5) DURING COLD PLANING OPERATION, A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION WILL REMAIN OPEN.
- (6) AFTER ANY COLD PLANING OPERATION, TEMPORARY STRIPING WILL BE REQUIRED BEFORE RETURNING ANY TRAFFIC ONTO EXCAVATED ROADWAY.

BRIDGE

- (1) THE CONTRACTOR SHOULD TAKE EXTREME CARE WHEN COLD PLANING THE EXISTING ASPHALT OFF BRIDGE DECK, SO AS NOT TO DAMAGE THE EXISTING DECK SEALANT AND/OR EXPANSION JOINT MEMBERS (STEEL PLATES, BARS, AND/OR HEADERS). IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL BE RESPONSIBLE FOR NECESSARY REPAIRS ON ALL DAMAGED MEMBERS TO THE SATISFACTION OF THE PROJECT ENGINEER AT NO ADDITIONAL COST.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ANY WORK REQUIRING LANE CLOSURES INCLUDING PAVEMENT MARKING OPERATIONS SHALL BE DONE AT NON-PEAK HOURS (9:00 A.M. TO 3:00 P.M. OR 7:00 P.M. TO 6:00 A.M.) UNLESS OTHERWISE DIRECTED BY THE TDOT OPERATIONS DISTRICT ENGINEER.
- (2) THE CONSTRUCTION SIGNS, FLAGMEN AND OTHER TRAFFIC CONTROL DEVICES WILL BE IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (3) ONE ADVANCE ROAD CONSTRUCTION AHEAD SIGN IS TO BE PLACED IN ALL LOCAL SIDE ROADS PRIOR TO THEIR INTERSECTION WITH THE PROPOSED PROJECT. LOCATION TO BE DETERMINED IN THE FIELD BY THE ENGINEER OR CONTRACTORS' SUPERINTENDENT.

WORK RESTRICTIONS

- (1) CONSTRUCTION ACTIVITIES SHALL NOT INTERFERE WITH SCHOOL OR WORK TRAFFIC EITHER AM OR PM OR AS DIRECTED BY THE ENGINEER.

UTILITIES

- (2) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATION AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

- (1) DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:

a. WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.

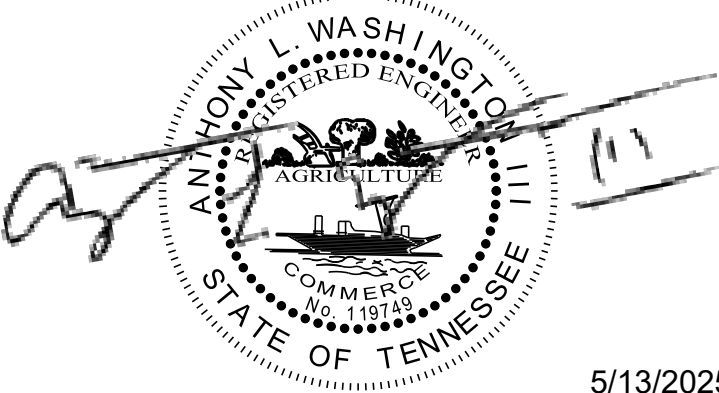
TSRR RAILROAD CROSSING NOTES

- (1) THE CONTRACTOR SHALL REPLACE ALL RR ADVANCE WARNING SIGNS, RXR PAVEMENT MARKINGS AND STOP LINES AT APPROACHES TO THIS GRADE CROSSING. THEY MUST SATISFY THE 2009 MUTCD STANDARDS AND REQUIREMENTS
- (2) ANY PAVEMENT PREPARATION, PAVEMENT INSTALLATION, PAVEMENT MARKINGS AND/OR RAILROAD WARNING SIGN INSTALLATION WITHIN 25- FEET OF THE NEAREST TSRR TRACK UTILIZING CONTRACTOR EQUIPMENT OR PERSONNEL WILL REQUIRE FULL TIME RAILROAD FLAGGING SERVICES OR AS OTHERWISE DETERMINED BY THE RAILROAD ONLY.
- (3) SEE AGREEMENT/STATE CONTRACT SPECIAL PROVISION 105C FOR WORK UP TO AND NEAR THE TSRR GRADE CROSSING REGARDING FLAGGING RULES AND INSURANCE REQUIREMENTS.
- (4) COLD PLANING AS CLOSE AS PRACTICAL TO THE FIELD SIDES OF THE CROSSING SURFACE, FOLLOWED BY RESURFACE UP TO AND LEVEL WITH THE TOP OF THE FIELD SIDES OF THE CROSSING SURFACE.
- (5) THE CROSSING SURFACE SHOULD BE ON THE SAME PLANE AS TSRR FOR A DISTANCE OF 30 INCHES OUTSIDE THE RAILS.
- (6) THE SURFACE OF THE HIGHWAY SHALL NOT BE MORE THAN 3 INCHES HIGHER OR LOWER THAN THE TOP OF THE TSRR AT A POINT 30 FEET FROM THE RAIL, UNLESS SUPERELEVATION MAKES A DIFFERENT LEVEL APPROPRIATE.
- (7) THE CONTRACTOR SHALL CONDUCT HIS WORK SO AS TO PROTECT THE TENNESSEE SOUTHERN RAILROAD TRACK FACILITIES AND PROPERTIES FROM ANY DAMAGE. THE WORK SHALL BE DONE IN ACCORDANCE WITH REGULATIONS STIPULATED BY TSRR RAILROAD SO AS TO MAINTAIN CLEARANCE AND NOT INTERRUPT TRAIN TRAFFIC IN ANY MANNER. THE CONTRACTOR SHALL NOT STORE ANY OF HIS CONSTRUCTION EQUIPMENT ON THE RAILROAD'S RIGHT-OF-WAY. THE CONTRACTOR SHALL NOTIFY AND COORDINATE HIS WORK AT THE RAILROAD CROSSING WITH THE FOLLOWING REPRESENTATIVE OF THE RAILROAD:

TERRY J. FRANK  
DIRECTOR OF ENGINEERING  
XORAIL, INC. (REPRESENTING TENNESSEE SOUTHERN RR)  
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ENVIRONMENTAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

- (4) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (9) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES

- (11) SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12) IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

- (15) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

- (1) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

ECOLOGY

- (2) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRE-CONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.
- (3) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.
- (4) ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

PROJECT COMMITMENTS

- (5) SEE PROJECT COMMITMENTS, SHEET 1B, FOR DETAILS RELATING TO SPECIAL ENVIRONMENTAL COMMITMENTS REQUIRED BY THIS PROJECT.

SCOPE OF WORK

- (6) THIS PROJECT INCLUDES MILL AND TLD OVERLAY, STRIPING, TRAFFIC CONTROL, AND BROOMING AND DEGRASSING SHOULDERS.

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

SEDIMENT CONTROL

- (6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.


GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (29) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.
- (35) WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36) ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.

- (37) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

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ENVIRONMENTAL  
NOTES



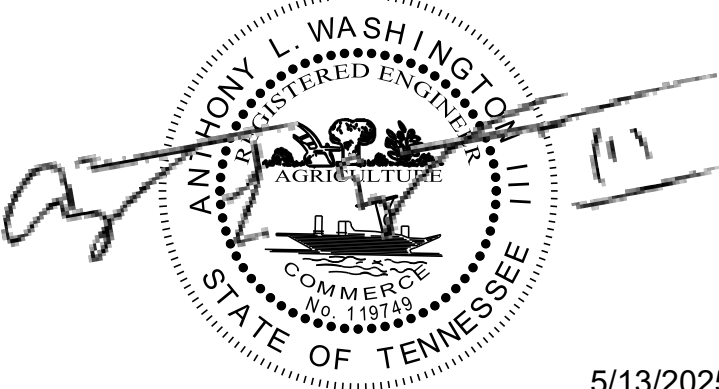
ENVIRONMENTAL NOTES (CONT.)

SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (44) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (47) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (52) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	60S006-S8-008	2D1

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DEPARTMENT OF TRANSPORTATION

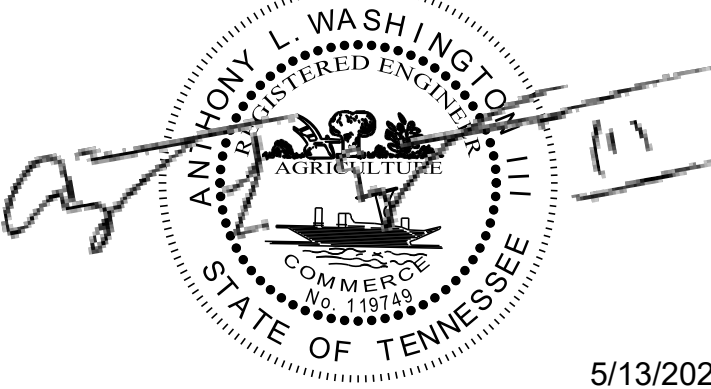
ENVIRONMENTAL  
NOTES

TRAFFIC CONTROL SIGN TABULATION (RESURFACING)					
M.U.T.C.D. SIGN NO.	LEGEND \ DESCRIPTION	SIZE IN INCHES		S.F.	TOTAL NUMBER REQUIRED
		L	x W		
G20-1	ROAD WORK NEXT 7 MILES	64"	x 24"	11	4
G20-2	END ROAD WORK	48"	x 24"	8	4
W4-2L	LEFT LANE ENDS SYMBOL	48"	x 48"	16	4
W4-2R	RIGHT LANE ENDS SYMBOL	48"	x 48"	16	4
W8-11	UNEVEN LANES	48"	x 48"	16	32
W8-15	GROOVED PAVEMENT	48"	x 48"	16	32
W8-15P	MOTORCYCLE PLAQUE	30"	x 24"	5	32
W13-1P	ADVISORY SPEED PLAQUE	30"	x 30"	6	32
W20-1	ROAD WORK AHEAD	48"	x 48"	16	8
W20-1	ROAD WORK 1 MILE	48"	x 48"	16	4
W20-1	ROAD WORK 1/2 MILE	48"	x 48"	16	4
W20-1	ROAD WORK 1000 FEET	48"	x 48"	16	4
W20-1	ROAD WORK 500 FEET	48"	x 48"	16	4
W20-5L	LEFT LANE CLOSED 1/2 MILE	48"	x 48"	16	4
W20-5L	LEFT LANE CLOSED 1500 FEET	48"	x 48"	16	4
W20-5R	RIGHT LANE CLOSED 1/2 MILE	48"	x 48"	16	4
W20-5R	RIGHT LANE CLOSED 1500 FEET	48"	x 48"	16	4
W21-2	FRESH OIL	48"	x 48"	16	4
W21-5	SHOULDER WORK	48"	x 48"	16	24
TOTAL					2675

PROPOSED GUARDRAIL (RESURFACING)						
SIDE		LOG MILE	GUARDRAIL		TERMINAL ANCHORS	
			EARTH PAD FOR TYPE 38 GR. END TREATMENT 705-04.09 (EACH)	GUARDRAIL REMOVED  706-01 (L.F.)	TYPE 38  MASH TL3 (46.875') 705-06.20 (EACH)	TYPE 21  MASH TL2 (21.875') 705-06.30 (EACH)
LT	RT					
	X	7.994	1	50	1	
TOTALS			1	50	1	

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	60S006-S8-008	2E

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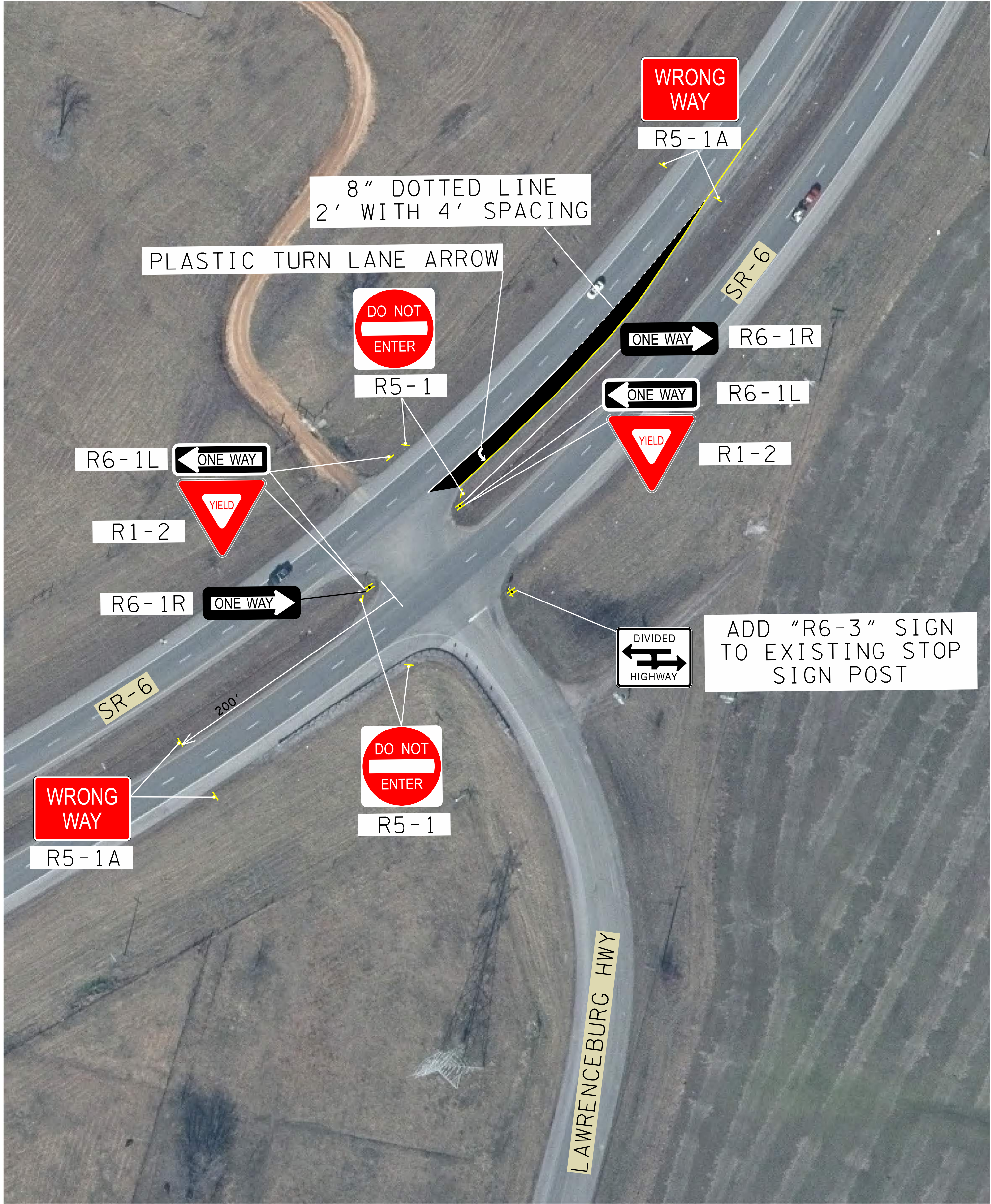
5/13/2025

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

TABULATED  
QUANTITIES



TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	60S006-S8-008	2F



STATE ROUTE 6  
LOG MILE 4.68 - 4.85  
MAURY COUNTY

REFERENCE STANDARD DRAWING T-M-4  
AND  
TDOT TRAFFIC SIGN AND PAVEMENT  
MARKING MANUAL FIGURE 2.14

FOR REFERENCE ONLY  
PROVIDED TDOT REGION 3 TRAFFIC



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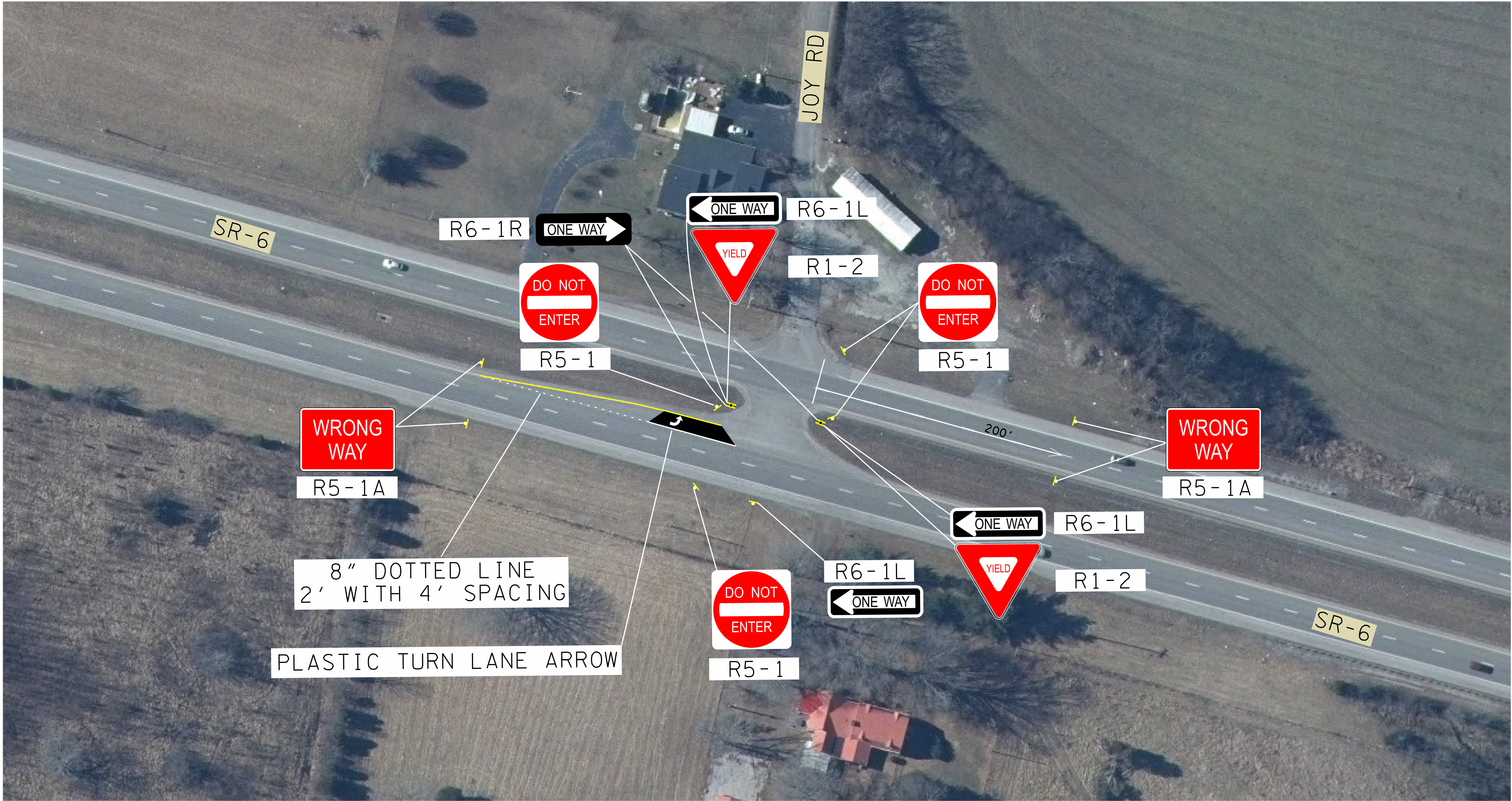
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

PAVEMENT  
MARKING  
IMPROVEMENTS

NOT TO SCALE



TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	60S006-S8-008	2F1



WRONG WAY  
R5-1A

8" DOTTED LINE  
2' WITH 4' SPACING

PLASTIC TURN LANE ARROW

STATE ROUTE 6  
LOG MILE 5.20 - 5.31  
MAURY COUNTY

REFERENCE STANDARD DRAWING T-M-4  
AND  
TDOT TRAFFIC SIGN AND PAVEMENT  
MARKING MANUAL FIGURE 2.15

FOR REFERENCE ONLY  
PROVIDED TDOT REGION 3 TRAFFIC

SEALED BY

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

PAVEMENT  
MARKING  
IMPROVEMENTS

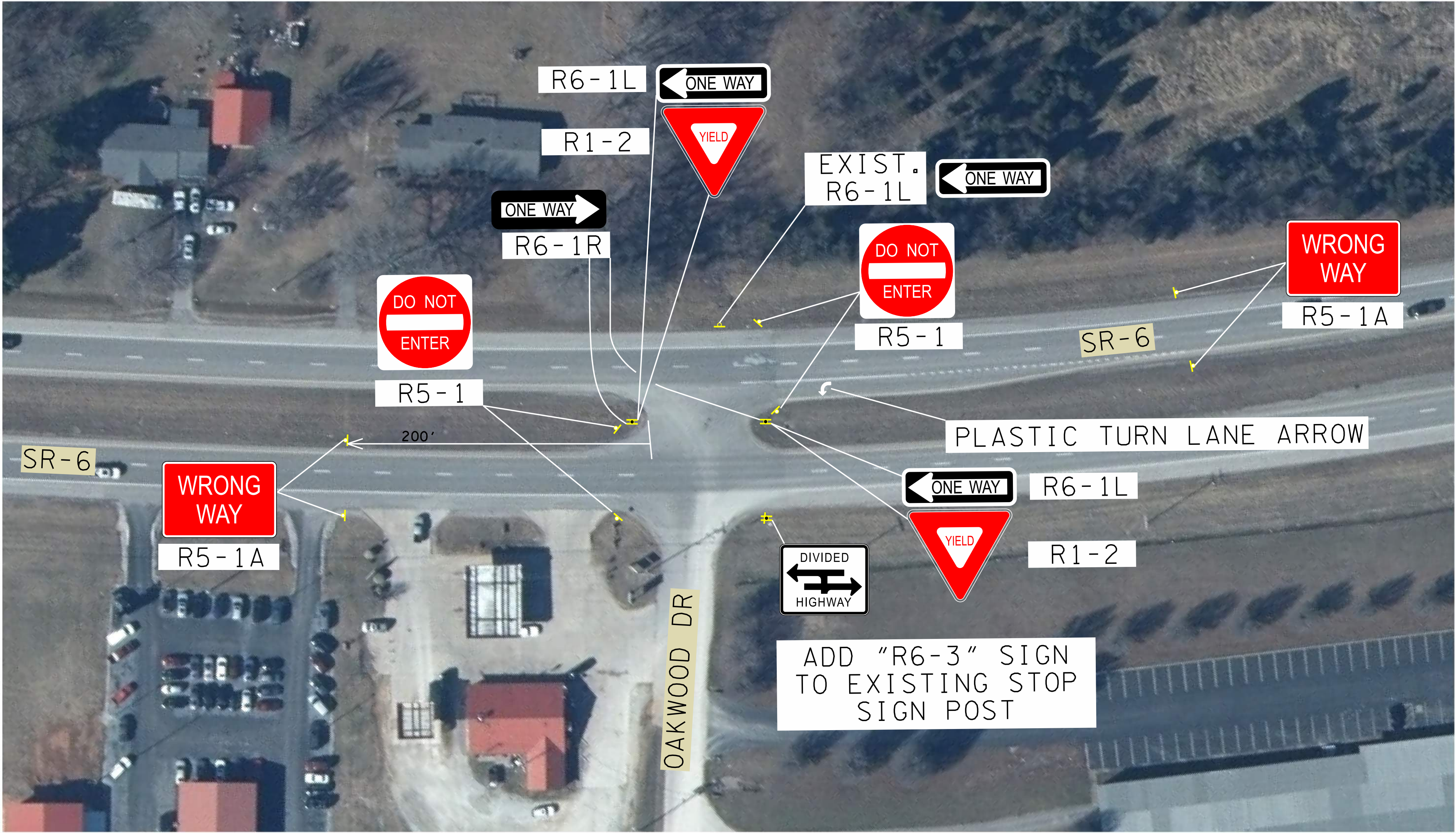
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\$\$\$\$SYTIME\$\$\$\$  
\$\$\$\$DGN\$SPEC\$\$\$\$



TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	60S006-S8-008	2F2



STATE ROUTE 6  
LOG MILE 6.00 - 6.15  
MAURY COUNTY

REFERENCE STANDARD DRAWING T-M-4  
AND  
TDOT TRAFFIC SIGN AND PAVEMENT  
MARKING MANUAL FIGURE 2.14

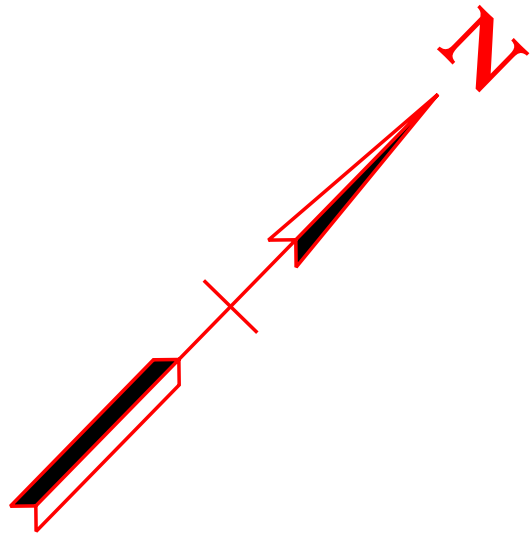
FOR REFERENCE ONLY  
PROVIDED TDOT REGION 3 TRAFFIC

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STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

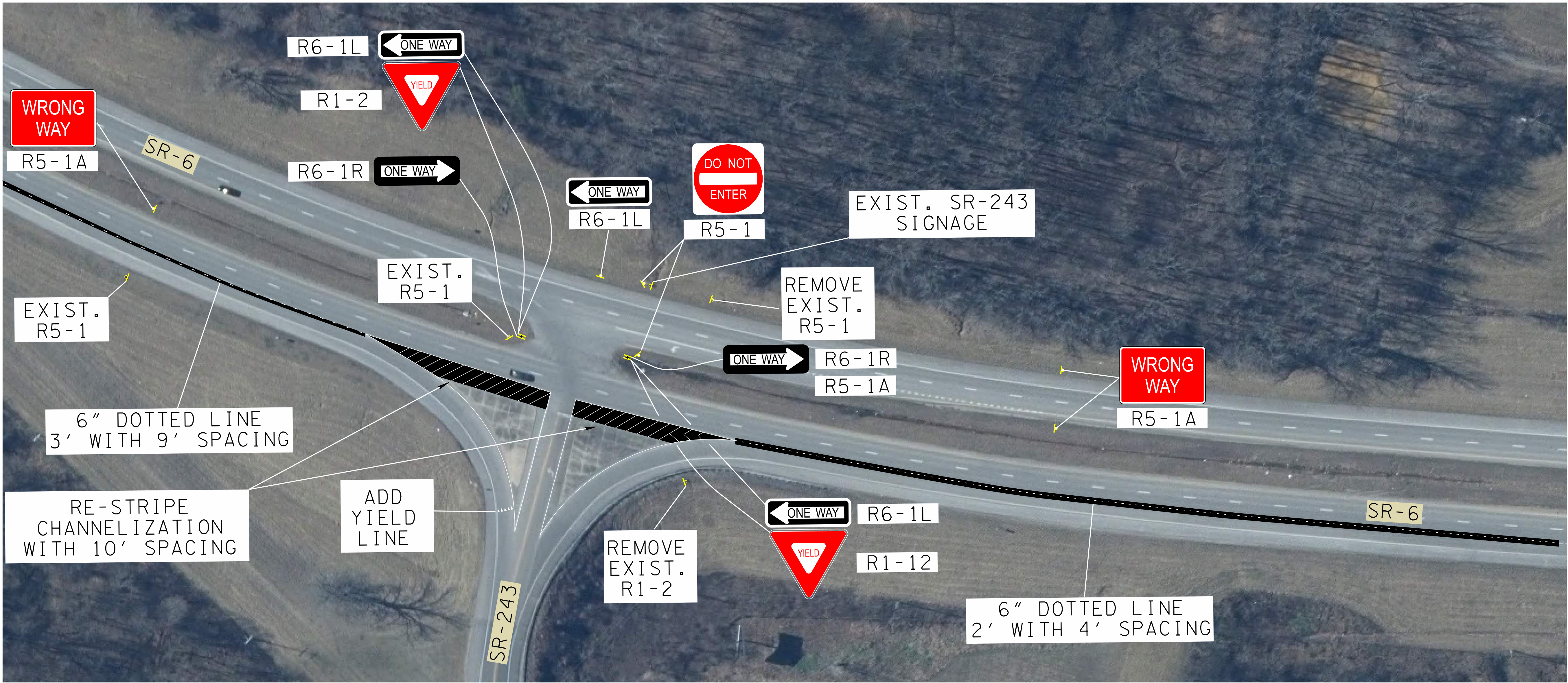
PAVEMENT  
MARKING  
IMPROVEMENTS

NOT TO SCALE



\$\$\$\$SYTIME\$\$\$\$  
\$\$\$\$DGN\$PEC\$\$\$\$





STATE ROUTE 6  
LOG MILE 6.25 - 6.55  
MAURY COUNTY

REFERENCE STANDARD DRAWING  
T-M-3 & T-M-6  
AND  
TDOT TRAFFIC SIGN AND PAVEMENT  
MARKING MANUAL FIGURE 2.14

FOR REFERENCE ONLY  
PROVIDED TDOT REGION 3 TRAFFIC

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	60S006-S8-008	2F3

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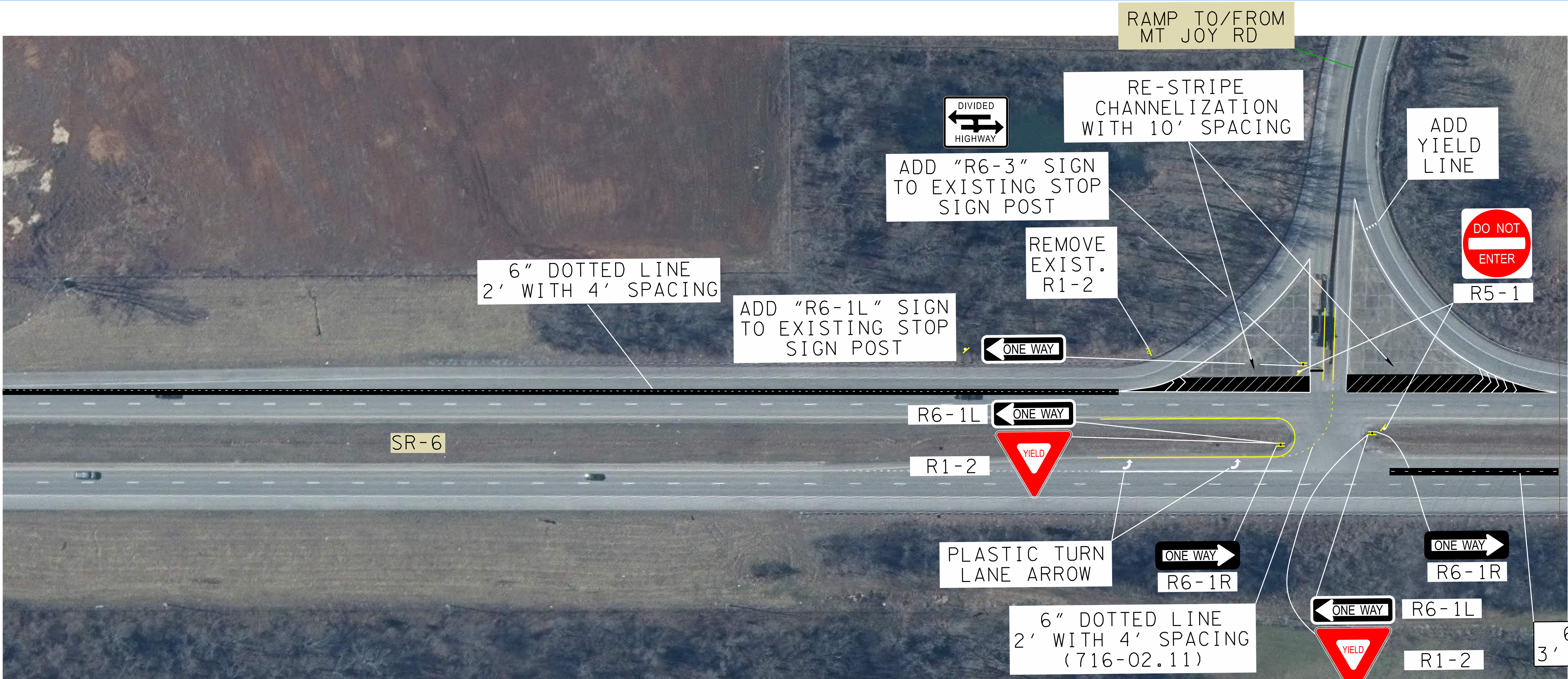
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

PAVEMENT  
MARKING  
IMPROVEMENTS

NOT TO SCALE



TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	60S006-S8-008	2F4



MATCH LINE A

STATE ROUTE 6  
LOG MILE 6.64 - 6.93  
MAURY COUNTY

REFERENCE STANDARD DRAWING  
T-M-3 & T-M-6, T-M-9A  
AND  
TDOT TRAFFIC SIGN AND PAVEMENT  
MARKING MANUAL FIGURE 2.15

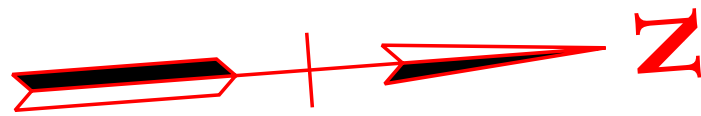
FOR REFERENCE ONLY  
PROVIDED TDOT REGION 3 TRAFFIC

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STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

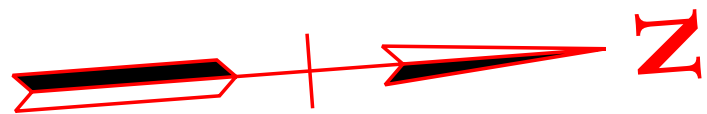
PAVEMENT  
MARKING  
IMPROVEMENTS

NOT TO SCALE





TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	60S006-S8-008	2F5



ESTIMATED PAVEMENT MARKING QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
713-15.01	REMOVAL OF EXISTING SIGN POST	EACH	3
713-16.21	SIGNS (R1-2)	EACH	10
713-16.22	SIGNS (R5-1)	EACH	16
713-16.23	SIGNS (R5-1A)	EACH	15
713-16.24	SIGNS (R6-1L)	EACH	13
713-16.25	SIGNS (R6-1R)	EACH	11
713-16.26	SIGNS (R6-3)	EACH	3
716-02.04	PLASTIC PAVEMENT MARKING (CHANNELIZATION STRIPING)	S.Y.	118
716-02.06	PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	5
716-02.08	PLASTIC PAVEMENT MARKING (8" DOTTED LINE)	L.F.	306
716-02.11	PLASTIC PAVEMENT MARKING (6" DOTTED LINE)	L.F.	97
716-04.12	PLASTIC PAVEMENT MARKING (YIELD LINE)	S.F.	27
716-12.05	ENHANCED FLATLINE THERMO PVMT MRKNG (6IN DOTTED LINE)	L.F.	4363

STATE ROUTE 6  
LOG MILE 6.93 - 7.18  
MAURY COUNTY

REFERENCE STANDARD DRAWING  
T-M-5 & T-M-6

FOR REFERENCE ONLY  
PROVIDED TDOT REGION 3 TRAFFIC

SEALED BY

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

PAVEMENT  
MARKING  
IMPROVEMENTS

NOT TO SCALE



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RIGHT-OF-WAY

- (12) EASEMENT REQUIRED FOR THE RAILROAD CROSSING IS TO BE OBTAINED BY THE UTILITIES ENGINEER BY PROVISIONS CONTAINED IN THE CROSSING AGREEMENT NEGOTIATED WITH THE RAILROAD.

UTILITY

- (1) THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES. ABOVE GRADE AND UNDERGROUND UTILITIES SHOWN WERE TAKEN FROM VISIBLE APPURTENANCES AT THE SITE, PUBLIC RECORDS, AND/OR MAPS PREPARED BY OTHERS. THEREFORE, RELIANCE UPON THE TYPE, SIZE, AND LOCATION OF UTILITIES SHOWN SHOULD BE DONE SO WITH THIS CIRCUMSTANCE CONSIDERED. DETAILED VERIFICATION OF EXISTENCE, LOCATION, AND DEPTH SHOULD ALSO BE MADE PRIOR TO ANY DECISION RELATIVE THERETO IS MADE. AVAILABILITY AND COST OF SERVICE SHOULD BE CONFIRMED WITH THE APPROPRIATE UTILITY COMPANY. IN TENNESSEE, IT IS A REQUIREMENT, PER “THE UNDERGROUND UTILITY DAMAGE PREVENTION ACT”, THAT ANYONE WHO ENGAGES IN EXCAVATION MUST NOTIFY ALL KNOWN UNDERGROUND UTILITY OWNERS, NO LESS THAN THREE (3) OR NOT MORE THAN TEN (10) WORKING DAYS PRIOR TO THE DATE OF THEIR INTENT TO EXCAVATE AND ALSO TO AVOID ANY POSSIBLE HAZARD OR CONFLICT. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800-351-1111 AS REQUIRED BY TCA 65-31-106 WILL BE REQUIRED.
- (2) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED ‘AROUND’ UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR’S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

UTILITY OWNERS

**GAS:**  
**ENBRIDGE (FORMERLY SPECTRA ENERGY)**  
1600 2<sup>ND</sup> AVE. SW, SUITE 16  
MINOT, ND 58701  
CONTACT: KELLY DALY  
OFFICE PHONE: 701 857 0813  
CELL PHONE: 701 340 5739  
Email: CROSSINGSUS@ENBRIDGE.COM

**ELECTRIC:**  
**MT. PLEASANT POWER SYSTEMS**  
123 NORTH MAIN STREET  
MT. PLEASANT, TN 38474  
CONTACT: AL KERSTIENS  
OFFICE PHONE: 931 379 3233  
CELL PHONE: 931 626 6109  
Email: AKERSTIENS@MOUNTPLEASANTPOWER.COM

**TELEPHONE:**  
**AT&T**  
116 SOUTH CANNON AVENUE  
MURFREESBORO, TN 37129  
CONTACT: KENNETH LEE KORNEGAY  
OFFICE PHONE: 615 848 2082  
CELL PHONE:  
Email: KK4096@ATT.COM

**WATER:**  
**MAURY COUNTY BOARD OF PUBLIC UTILITIES**  
765 NEW LEWISBURG HWY  
COLUMBIA, TN 38401  
CONTACT: TODD SHULTZ  
OFFICE PHONE: 931 381 8900  
OFFICE PHONE: 931 375 1161  
Email: TSHULTZ@MYMCWS.COM  
CONTACT: DAWN BAXTER  
OFFICE PHONE: 931 381 8900  
OFFICE PHONE: 931 375 1161  
Email: DBAXTER@MYMCWS.COM

**FOC:**  
**LUMEN (FORMERLY CENTURYLINK)**  
520 WHALEY ST.  
COLUMBIA, SC 29201  
CONTACT: JOHN BOEDEKER  
OFFICE PHONE:  
CELL PHONE: 512 334 8351  
Email: JOHN.BOEDEKER@LUMEN.COM  
2530 PERIMETER PLACE DRIVE  
NASHVILLE, TN 37214  
CONTACT: JEFFREY CANNON  
OFFICE PHONE: 615 263 1128  
CELL PHONE: 615 419 6617  
Email: JEFFEREY.CANNON@LUMEN.COM

**ELECTRIC:**  
**TENNESSEE VALLEY AUTHORITY.**  
1101 MARKET STREET MR-4G  
CHATTANOOGA, TN 37402  
CONTACT: STEPHEN WILLIAMS  
OFFICE PHONE:  
CELL PHONE: 662 255 6272  
Email: SEWILLIAMS@TVA.GOV

**GAS:**  
**HORTON HIGHWAY UTILITY DISTRICT**  
102 S. MAIN STREET, P.O. BOX 85  
CHAPEL HILL, TN 37034  
CONTACT: MARK CURTIS  
OFFICE PHONE: 931 364 2283  
CELL PHONE:  
Email: UTILITYCO@UNITED.NET

**ELECTRIC:**  
**DUCK RIVER ELECTRIC MEMBERSHIP COR**  
1411 MADISON STREET  
SHELBYVILLE, TN 37160  
CONTACT: SCOTT DAHLSTROM  
OFFICE PHONE: 931 680 5840  
CELL PHONE: 931 703 0248  
Email: SDAHLSTROM@DREMC.COM

**FOC:**  
**COLUMBIA BROADBAND**  
201 PICKENS LANE  
COLUMBIA, TN 38401  
CONTACT: BRAD TEBBEN  
OFFICE PHONE: 931 375 7682  
CELL PHONE:  
Email: BRAD.TEBBEN@CPWS.COM

**WATER, SEWER, GAS**  
**MT. PLEASANT UTILITES**  
100 PUBLIC SQUARE  
MT. PLEASANT, TN 38474  
CONTACT: TED HOWELL  
OFFICE PHONE: 931 379 7717  
CELL PHONE: 931 774 4917  
Email: THOWELL@MTPLEASANT-TN.GOV

CONTACT: DALE BROWN  
OFFICE PHONE:  
CELL PHONE: 931 774 6343  
Email: DBROWN@MTPLEASANT-TN.GOV

**CABLE:**  
**CHARTER COMMUNICATIONS**  
1850 BUSINESS PARK DR.  
CLARKSVILLE, TN 37040  
CONTACT: DAVID ROBINSON  
OFFICE PHONE:  
CELL PHONE: 347  
Email: DAVID.G.ROBINSON@CHARTER.COM

1757 NORTH POINTE DR.  
COLUMBIA, TN 38401  
CONTACT: WILL JAGGERS  
OFFICE PHONE:  
CELL PHONE:  
Email: WILL.JAGGERS@CHARTER.COM

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	60S006-S8-008	3

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5/13/2025

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

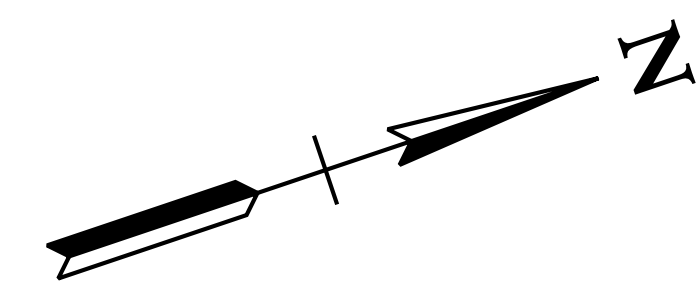
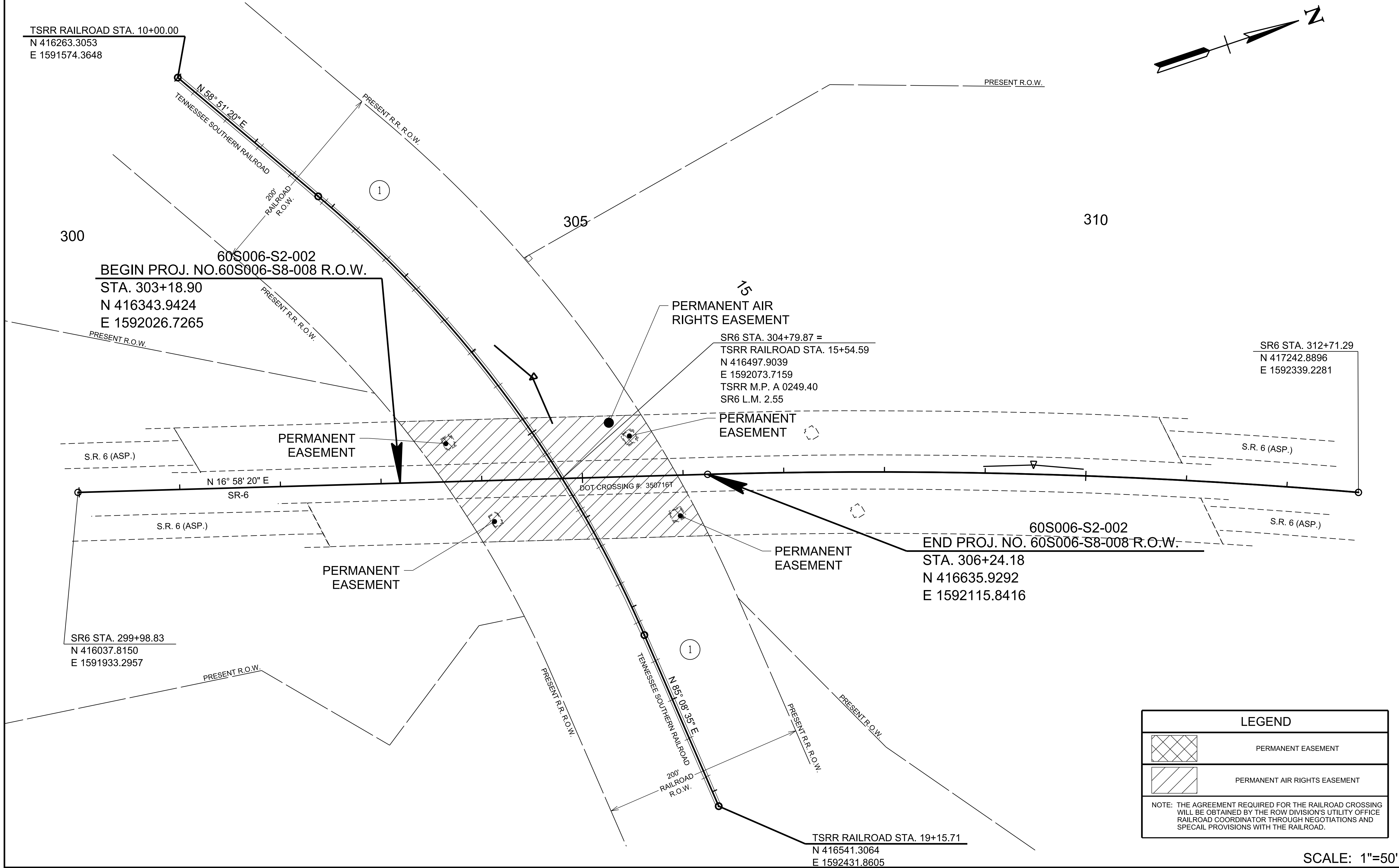
RIGHT-OF-WAY  
NOTES,  
UTILITY NOTES  
AND  
UTILITY OWNERS



R.O.W. ACQUISITION TABLE																		
TRACT NO.	PROPERTY OWNERS	COUNTY RECORDS				TOTAL AREA (ACRES)			AREA TO BE ACQUIRED (ACRES)			AREA REMAINING (ACRES)		EASEMENT (ACRES)				
		TAX MAP NO.	PARCEL NO.	DEED DOCUMENT REFERENCE		LEFT	RIGHT	TOTAL	LEFT	RIGHT	TOTAL	LEFT	RIGHT	PERM PIER FOOTING	SLOPE	CONST	AIR RIGHTS	PERM RAILROAD
				BOOK	PAGE													
1	TENNESSEE SOUTHERN RAILROAD COMPANY																	
ACQUISITION TOTALS (ACRES)																		

\*\* CONFIRMATION OF EASEMENT CORRIDOR BY RAILROAD ENTITY

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	60S006-S8-008	3A



LEGEND

PERMANENT EASEMENT

PERMANENT AIR RIGHTS EASEMENT

NOTE: THE AGREEMENT REQUIRED FOR THE RAILROAD CROSSING WILL BE OBTAINED BY THE ROW DIVISION'S UTILITY OFFICE RAILROAD COORDINATOR THROUGH NEGOTIATIONS AND SPECAIL PROVISIONS WITH THE RAILROAD.

SEALED BY

ANTHONY L. WASHINGTON

REGISTERED ENGINEER

COMMERCIAL

NO. 11574

STATE OF TENNESSEE

5/13/2025

COORDINATES ARE NAD 83(2011). ARE DATUM ADJUSTED BY THE FACTOR OF 1.00007 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988 WITH GEOID 12B.

STATE OF TENNESSEE

DEPARTMENT OF TRANSPORTATION

PROPERTY MAP

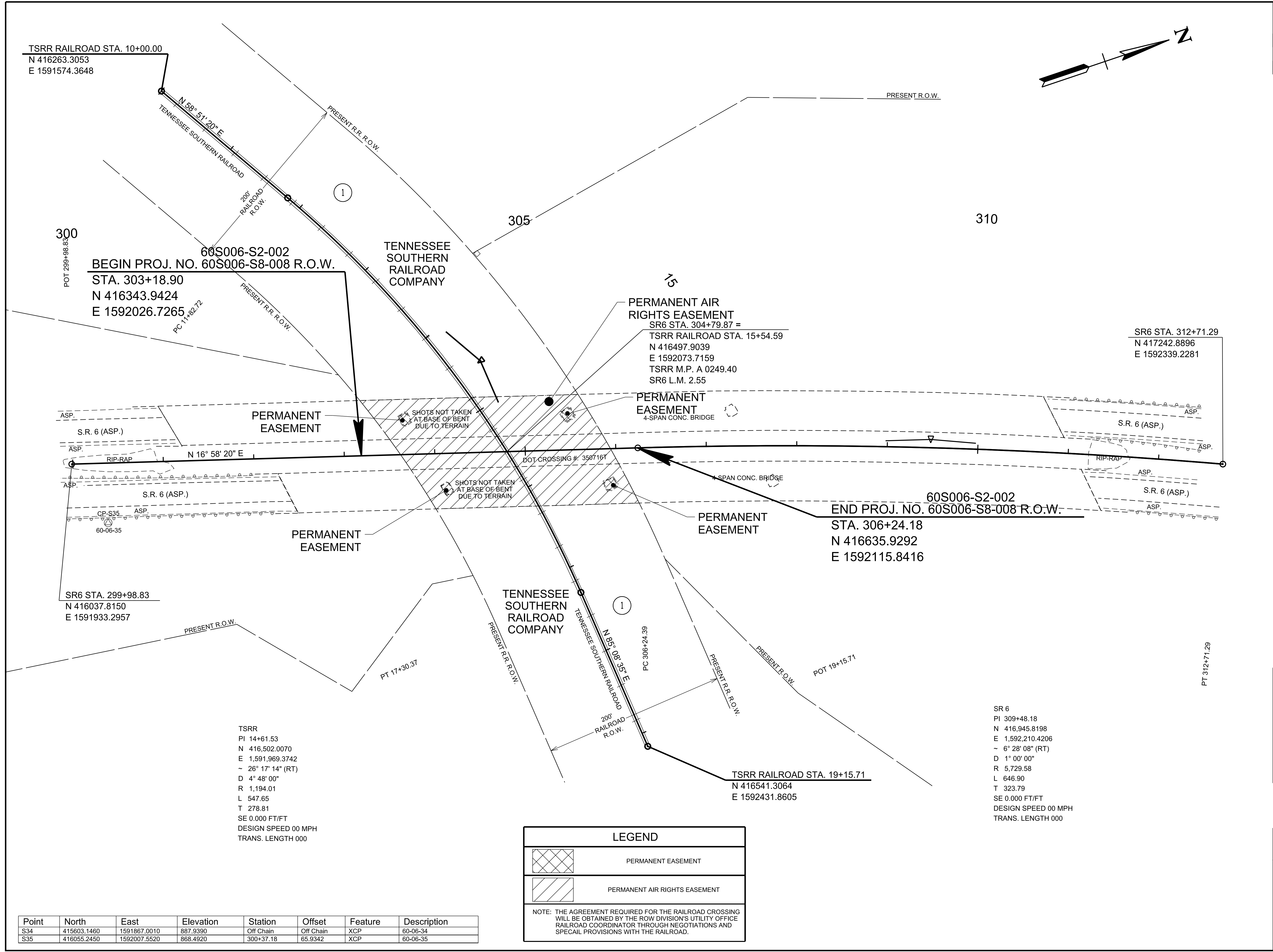
AND

RIGHT-OF-WAY



ACQUISITION

TABLE

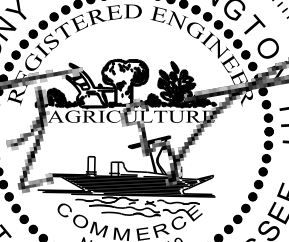
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	60S006-S8-008	4



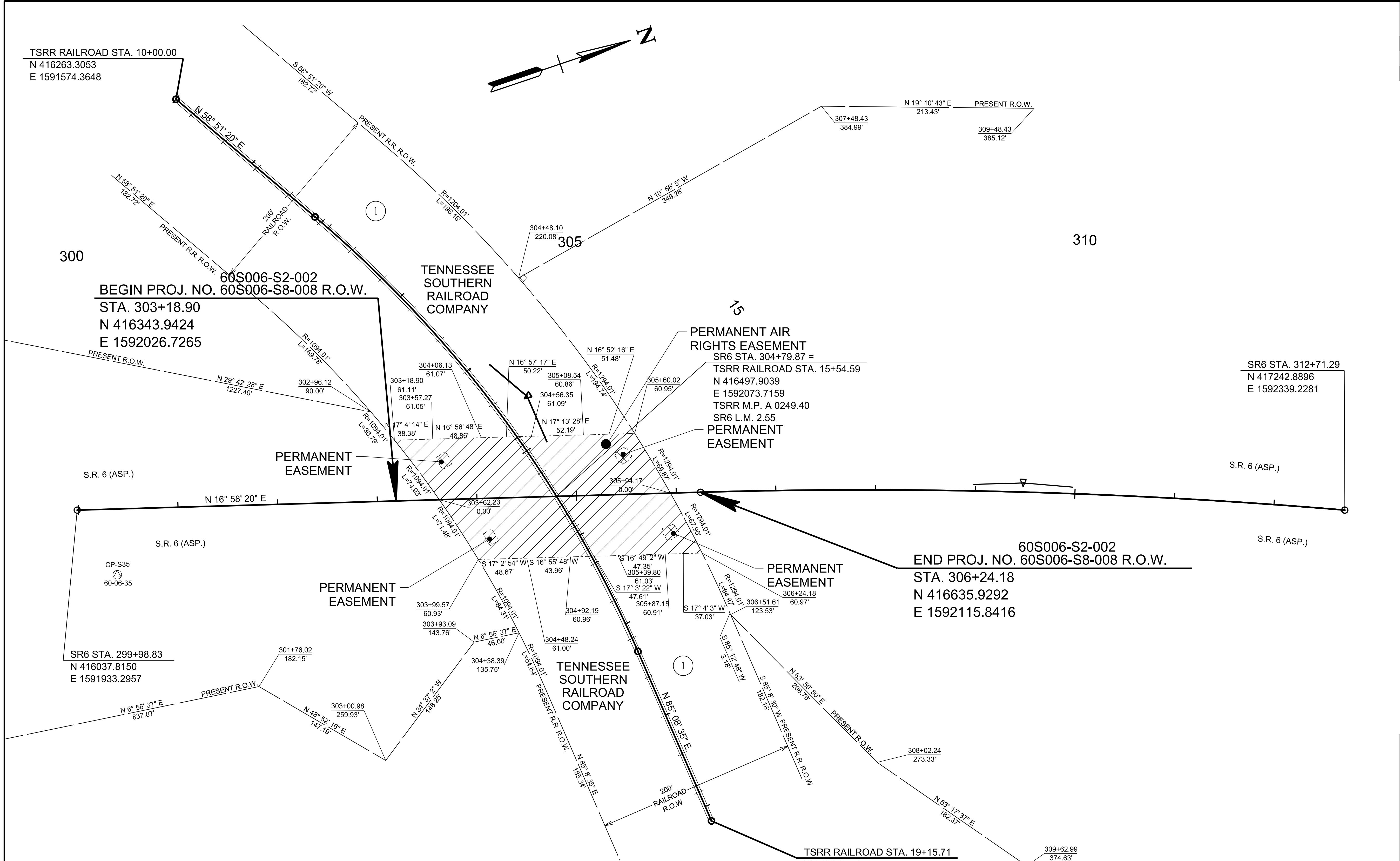
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S35	416055.2450	1592007.5520	868.4920	300+37.18	65.9342	XCP	60-06-35

LEGEND	
	PERMANENT EASEMENT
	PERMANENT AIR RIGHTS EASEMENT

NOTE: THE AGREEMENT REQUIRED FOR THE RAILROAD CROSSING WILL BE OBTAINED BY THE ROW DIVISION'S UTILITY OFFICE RAILROAD COORDINATOR THROUGH NEGOTIATIONS AND SPECIAL PROVISIONS WITH THE RAILROAD.

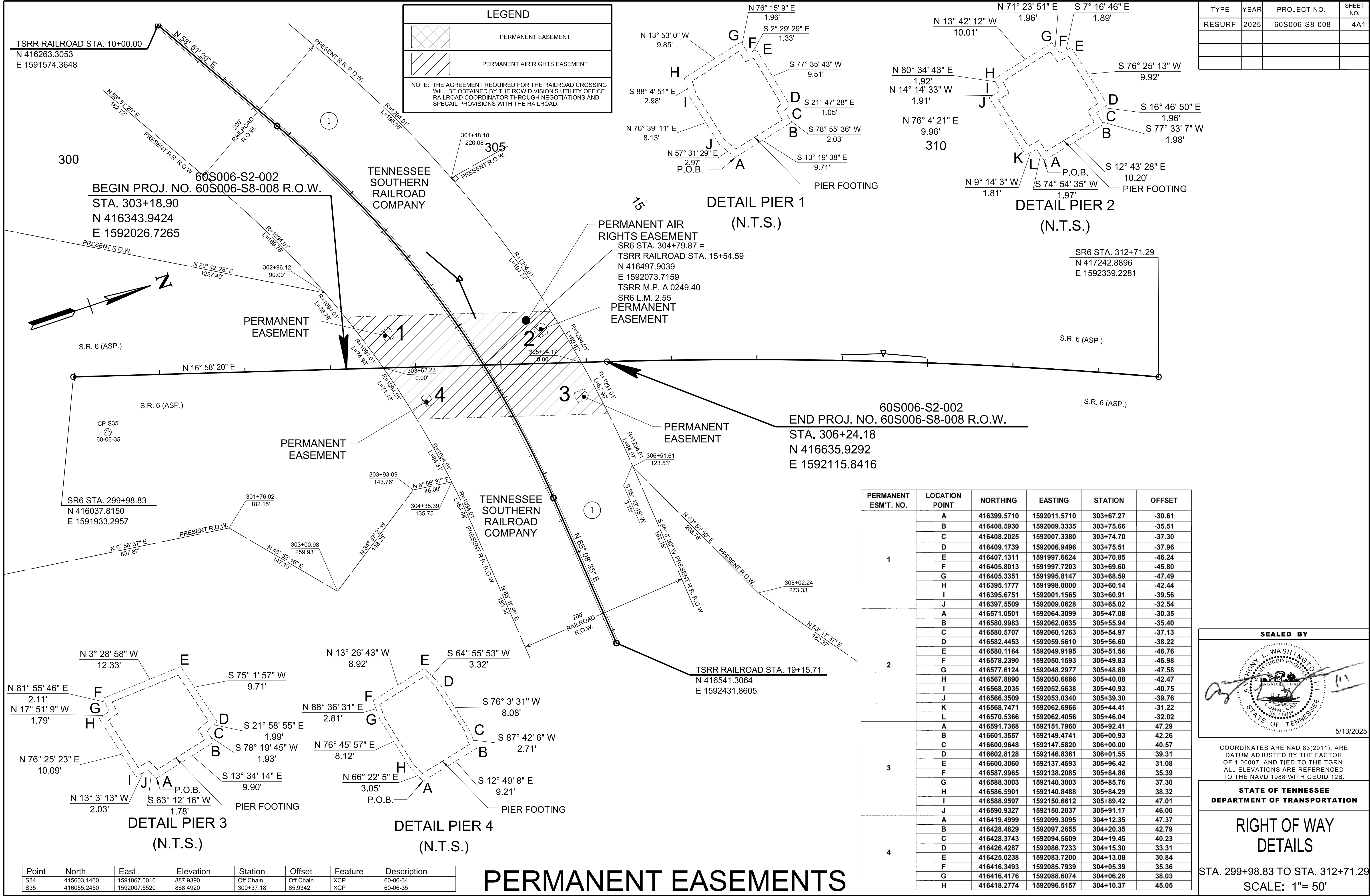
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<p>COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00007, AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988 WITH GEOID 12B.</p>	
<p style="text-align: center;"><b>STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION</b></p>	
<p style="text-align: center; font-size: 2em;"><b>PRESENT LAYOUT</b></p>	
<p>STA. 299+98.83 TO STA. 312+71.29</p> <p style="text-align: center; font-size: 1.5em;"><b>SCALE: 1"= 50'</b></p>	

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	60S006-S8-008	4A



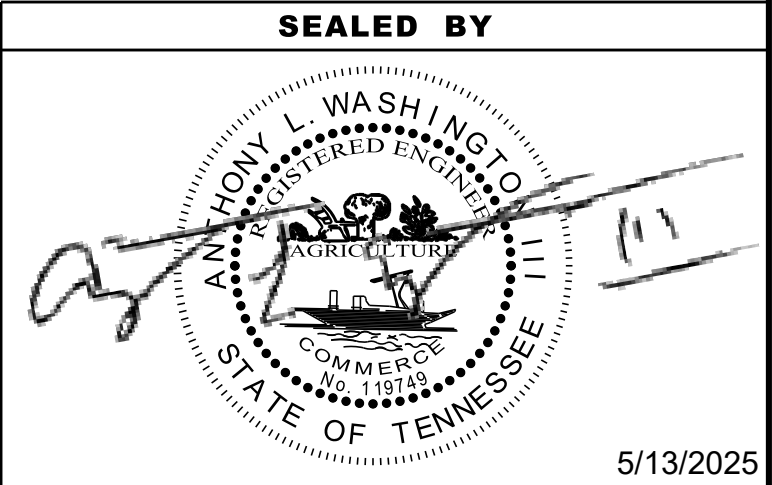


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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	60S006-S8-008	4A1

PERMANENT ESM'T. NO.	LOCATION POINT	NORTHING	EASTING	STATION	OFFSET
1	A	416399.5710	1592011.5710	303+67.27	-30.61
	B	416408.5930	1592009.3335	303+75.66	-35.51
	C	416408.2025	1592007.3380	303+74.70	-37.30
	D	416409.1739	1592006.9496	303+75.51	-37.96
	E	416407.1311	1591997.6624	303+70.85	-46.24
	F	416405.8013	1591997.7203	303+69.60	-45.80
	G	416405.3351	1591995.8147	303+68.59	-47.49
	H	416395.1777	1591998.0000	303+60.14	-42.44
	I	416395.6751	1592001.1565	303+60.91	-39.56
	J	416397.5509	1592009.0628	303+65.02	-32.54
2	A	416571.0501	1592064.3099	305+47.08	-30.35
	B	416580.9983	1592062.0635	305+55.94	-35.40
	C	416580.5707	1592060.1263	305+54.97	-37.13
	D	416582.4453	1592059.5610	305+56.60	-38.22
	E	416580.1164	1592049.9195	305+51.56	-46.76
	F	416578.2390	1592050.1593	305+49.83	-45.98
	G	416577.6124	1592048.2977	305+48.69	-47.58
	H	416567.8890	1592050.6686	305+40.08	-42.47
	I	416568.2035	1592052.5638	305+40.93	-40.75
	J	416566.3509	1592053.0340	305+39.30	-39.76
3	K	416568.7471	1592062.6966	305+44.41	-31.22
	L	416570.5366	1592062.4056	305+46.04	-32.02
	A	416591.7368	1592151.7960	305+92.41	47.29
	B	416601.3557	1592149.4741	306+00.93	42.26
	C	416600.9648	1592147.5820	306+00.00	40.57
	D	416602.8128	1592146.8361	306+01.55	39.31
	E	416600.3060	1592137.4593	305+96.42	31.08
	F	416587.9965	1592138.2085	305+84.86	35.39
	G	416588.3003	1592140.3003	305+85.76	37.30
	H	416586.5901	1592140.8488	305+84.29	38.32
4	I	416588.9597	1592150.6612	305+89.42	47.01
	J	416590.9327	1592150.2037	305+91.17	46.00
	A	416419.4999	1592099.3095	304+12.35	47.37
	B	416428.4829	1592097.2655	304+20.35	42.79
	C	416428.3743	1592094.5609	304+19.45	40.23
	D	416426.4287	1592086.7233	304+15.30	33.31
	E	416425.0238	1592083.7200	304+13.08	30.84
	F	416416.3493	1592085.7939	304+05.39	35.36



COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00007 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988 WITH GEOID 12B.

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**RIGHT OF WAY  
DETAILS**

STA. 299+98.83 TO STA. 312+71.29  
SCALE: 1"= 50'



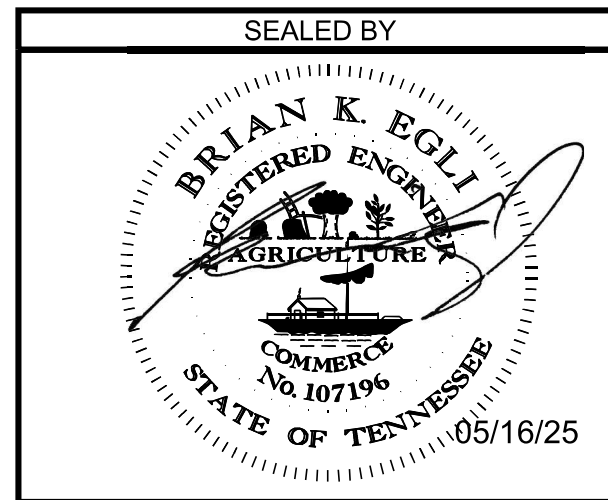


PROJECT NO.		YEAR		SHEET NO.	
60S006-M3-009		2025		B-1	
REVISIONS					
NO.	DATE	BY	BRIEF DESCRIPTION		
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<u>INDEX OF DRAWINGS</u>	<u>DWG. NO.</u>	<u>LAST REV. DATE</u>
SIGNATURE SHEET	STRUCTURE-SIGN 1	
INDEX OF DRAWINGS	B1	
BRIDGE TABULATION, ESTIMATED QUANTITIES, REPAIRS NOTES AND DETAILS	B2	
PLAN VIEW REPAIRS LOCATIONS	B3	
PHASE CONSTRUCTION	B4	
VERTICAL CLEARANCES AND RAILROAD NOTES	B5	
PLAN VIEW REPAIRS LOCATIONS	B6	
PHASE CONSTRUCTION	B7	
TYPE 1 THIN EPOXY OVERLAY NOTES	B8	
EXPANSION JOINT DETAILS		
AT APPROACHES PAVEMENTS ENDS	B9	
EXPANSION JOINT DETAILS		
AT APPROACHES PAVEMENTS ENDS	B10	

INDEX OF REFERENCE DRAWINGS	DWG. NO.
LAYOUT OF THE BRIDGE	M-324-1
SUPERSTRUCTURE	M-324-5
SUPERSTRUCTURE	M-324-6
LAYOUT OF THE BRIDGE	M-324-33
SUPERSTRUCTURE	M-324-36
SUPERSTRUCTURE	M-324-38
REINFORCED CONCRETE PAVEMENT AT THE BRIDGE ENDS	STD-1-5

PIN NO.:	122522.00	
DESIGN BY:		DATE: / /
DRAWN BY:	SILESHI ERGICHO	DATE: 11/24
SUPERVISED BY:	KEVIN MARTINKO	DATE: 11/24
CHECKED BY:		DATE: / /



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
INDEX OF DRAWINGS  
60-SR6-2.51 RT. & 60-SR6-2.48 LT.  
OVER TENNESSEE SOUTHERN  
RAILROAD AND BIG BIGBY CREEK  
60-SR6-5.85 RT. AND LT.  
OVER BIG BIGBY CREEK  
MAURY COUNTY  
2025

PROJECT NO.		YEAR		SHEET NO.	
60S006-M3-009		2025		B-2	
REVISIONS					
NO.	DATE	BY	BRIEF DESCRIPTION		
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TABULATION OF BRIDGE RELATED WORK AND ESTIMATED QUANTITIES						
LOCATION OF BRIDGE AND BRIDGE NUMBER	REFERENCE DRAWINGS TO BE PRINTED WITH CONTRACT DRAWINGS	TYPE OF WORK	604-10.44 EXPANSION JOINT REPAIRS L.F.	604-10.50 BRIDGE DECK REPAIRS (PARTIAL DEPTH OF SLAB) S.Y.	604-10.53 CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PAVEMENT) S.Y.	617-04.01 TYPE 1 THIN EPOXY OVERLAY (EPOXY-URETHANE) S.Y.
60-SR6-2.51 RT. OVER BIG BIGBY CREEK AND TENNESSEE SOUTHERN RAILROAD (60SR0060083)	M-324-1 M-324-5 STD-1-5	EXPANSION JOINT REPAIRS BRIDGE DECK REPAIRS CONCRETE REPAIR TYPE 1 THIN EPOXY OVERLAY	94	10	15	4649
60-SR6-2.48 LT. OVER BIG BIGBY CREEK AND TENNESSEE SOUTHERN RAILROAD (60SR0060084)	M-324-1 M-324-6 STD-1-5	EXPANSION JOINT REPAIRS BRIDGE DECK REPAIRS CONCRETE REPAIR TYPE 1 THIN EPOXY OVERLAY	94	10	15	4266
60-SR6-5.58 RT. OVER BIG BIGBY CREEK (60SR0060009)	M-324-33 M-324-36 STD-1-5	EXPANSION JOINT REPAIRS	116			
60-SR6-5.58 LT. OVER BIG BIGBY CREEK (60SR0060010)	M-324-33 M-324-38 STD-1-5	EXPANSION JOINT REPAIRS	116			
TOTAL			420	20	30	8915

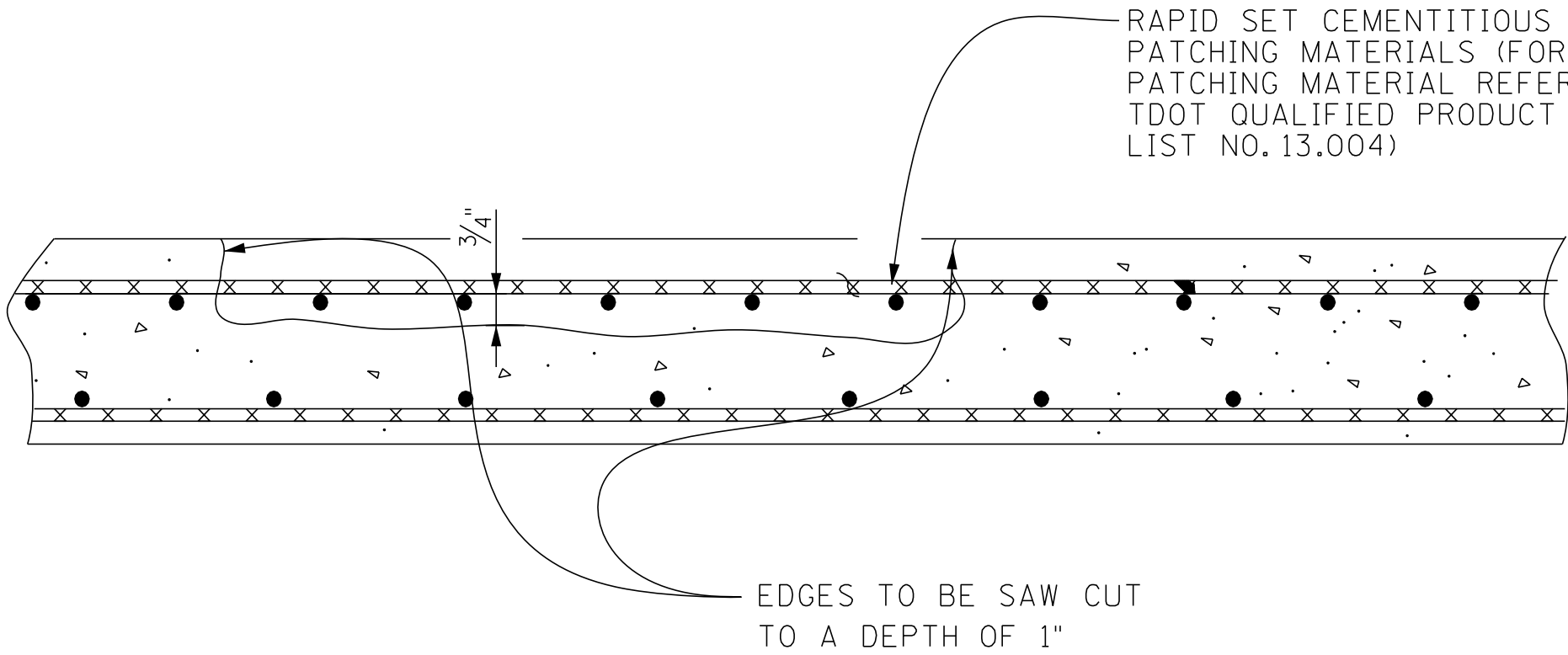
BRIDGE DECK REPAIR MATERIAL

ALL REPAIRS SHALL USE ONLY THE NON-MAGNESIUM PHOSPHATE QUICK SET PATCHING MATERIAL FROM THE TDOT QUALIFIED PRODUCTS LIST: 13.004-RAPID SET CEMENTITIUOS PATCHING MATERIALS. SHALL BE APPLIED ACCORDING TO MANUFACTURER’S SPECIFICATIONS.

PATCHING MATERIAL SHOULD BE ABLE TO ACHIEVE A MINIMUM 3000PSI AT 18 HOURS. PATCHING MATERIAL SHALL REACH 3000PSI BEFORE OPENING TO THE TRAFFIC.

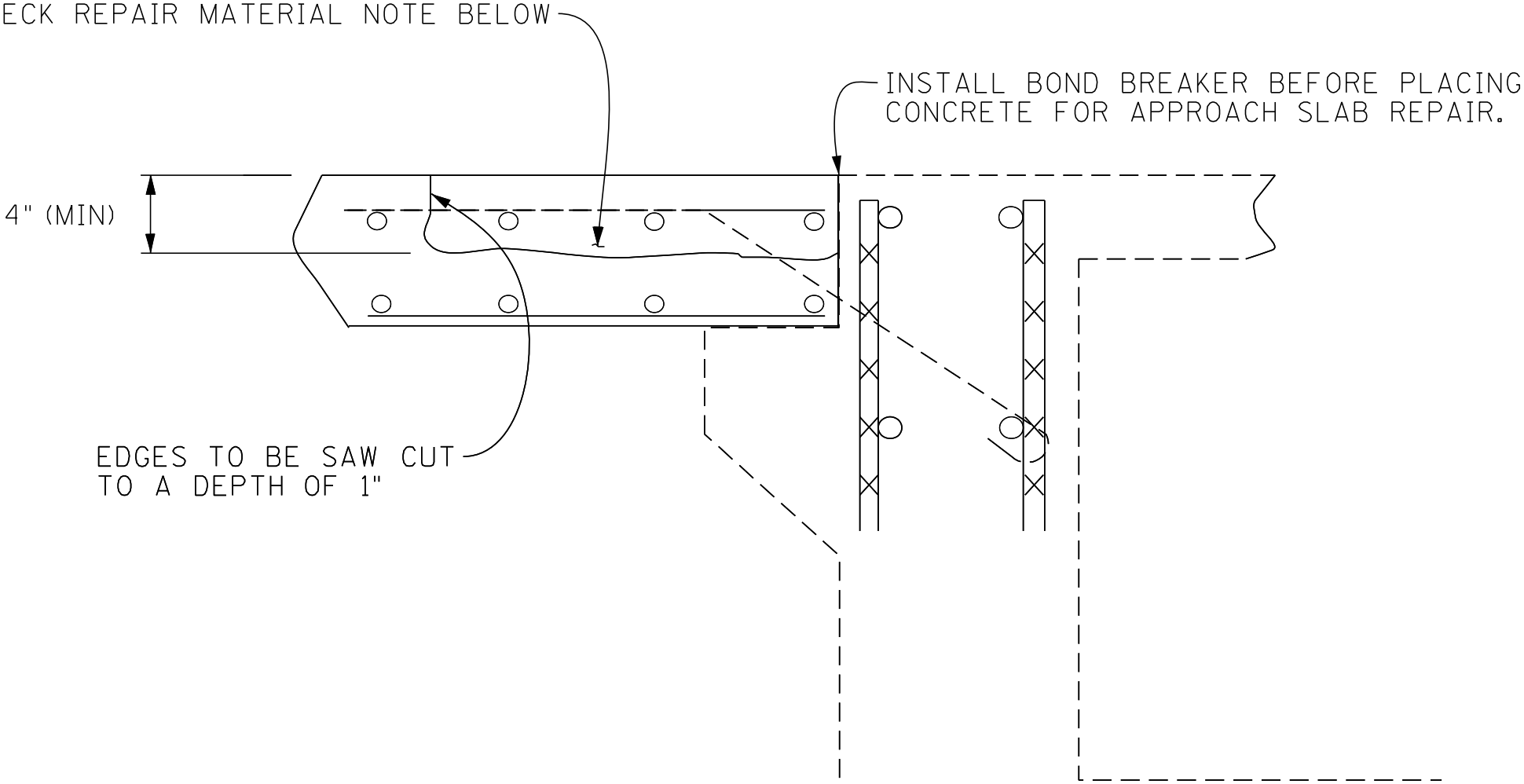
POWER DRIVEN HAND TOOLS:

POWER DRIVEN HAND TOOLS USED FOR THE REMOVAL OF UNSOUND CONCRETE IN MAKING PARTIAL AND FULL DEPTH REPAIRS ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:  
(1) PARTIAL DEPTH REPAIRS; PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 60 LB. CLASS SHALL NOT BE USED. ALSO TRAFFIC CONTROL SHALL BE SET UP DURING PARTIAL DEPTH REPAIRS OVER TRAFFIC.  
(2) FULL DEPTH REPAIRS; PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 90 LB. CLASS SHALL NOT BE USED. ALSO ALL DECK REPAIR OVER BEAMS WILL BE RESTRICTED TO 60 LB. PNEUMATIC HAMMERS.  
(3) CHIPPING HAMMERS OF THE 15 LB. CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL.



SKETCH SHOWING DECK REPAIR

SEE BRIDGE DECK REPAIR MATERIAL NOTE BELOW



CONCRETE APPROACH PAVEMENT REPAIR DETAILS:

NOTE: REMOVE CONCRETE TO A DEPTH OF ¾" MINIMUM BELOW EXISTING REINFORCING STEEL OR TO SOUND CONCRETE. CONCRETE REMOVAL SHOULD BE TO A MINIMUM DEPTH OF 4". DEPTH FOR CONCRETE REMOVAL SHOULD NOT BE MORE THAN 6".

NOTE: COST OF SAW CUTTING, CONCRETE REMOVAL, COMPLETELY CLEANING EXPOSED REINFORCING STEEL, LABOR, AND ANY MISCELLANEOUS MATERIALS NECESSARY TO COMPLETE THE REPAIRS AS SHOWN SHALL BE INCLUDED IN ITEM NO. 604-10.53 CONCRETE REPAIR S.Y.

NOTE: ITEM NO. 604-10.53, CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PVMT.) MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.

NOTE: PARTIAL DEPTH DECK REPAIR ONLY REMOVE CONCRETE IN ALL DELAMINATED AREAS TO A DEPTH OF ¾" BELOW THE TOP BAR OF THE TOP MAT OF REINFORCING STEEL.

NOTE: PARTIAL DEPTH REPAIRS ALL REINFORCING STEEL IN THE AREAS OF THE DECK REPAIRS SHALL BE COMPLETELY CLEANED. CLEANING SHALL BE DONE PRIOR TO PLACING NEW CONCRETE. AREAS OF CONCRETE REMOVAL SHALL BE DESIGNATED BY PERSONNEL FROM THE HEADQUARTERS, BRIDGE INSPECTION AND REPAIR OFFICE. INSPECTIONS TO DETERMINE AREAS OF DECK REPAIR SHALL BE SCHEDULED WITH THE BRIDGE REPAIR OFFICE AT LEAST (3) DAYS IN ADVANCE.

THE COST OF CONCRETE, CONCRETE REMOVAL, LABOR, CLEANING REBAR AND ANY MISCELLANEOUS MATERIAL REQUIRED SHALL BE PAID FOR UNDER ITEM NO. 604-10.50 BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB), S.Y.

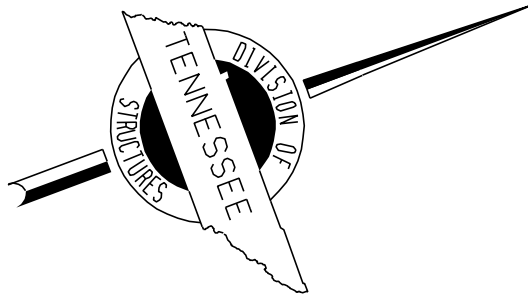
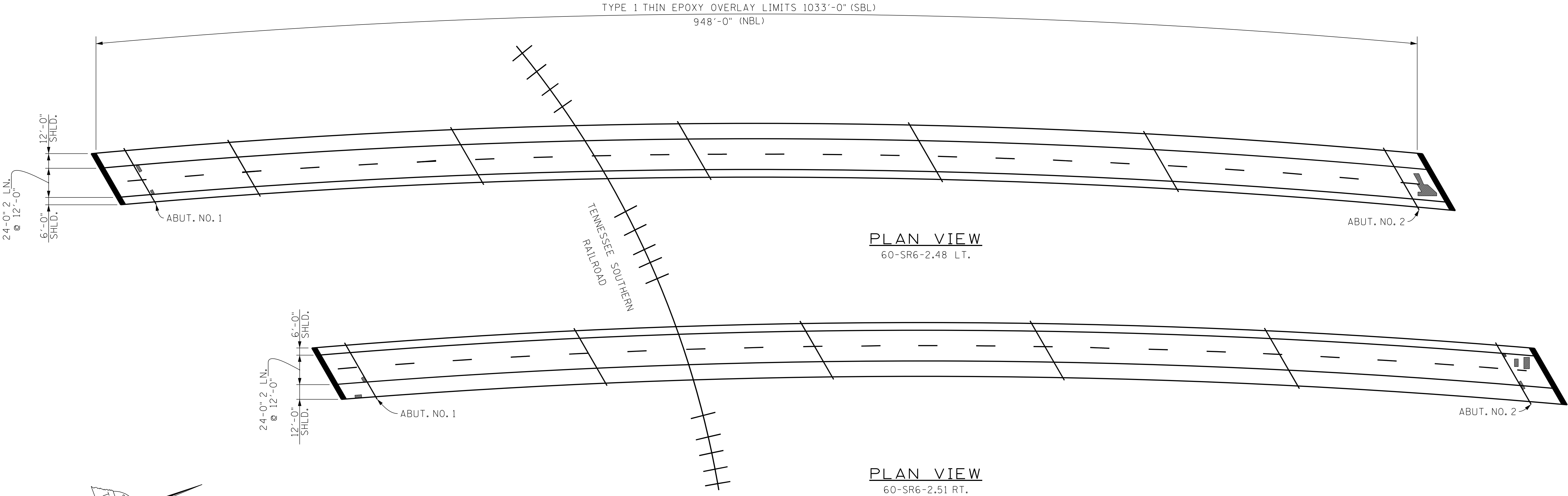
NOTE: ITEM NOS. 604-10.50, BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB) SHALL BE BID THIS MAY BE INCREASED, DECREASED OR ELIMINATED AS APPROVED BY THE ENGINEER.

PIN NO:	122522.00	DATE:	/ /
DESIGN BY:		DATE:	11/24
DRAWN BY:	SILESHE ERGICHO	DATE:	11/24
SUPERVISED BY:	KEVIN MARTINKO	DATE:	/ /
CHECKED BY:		DATE:	/ /

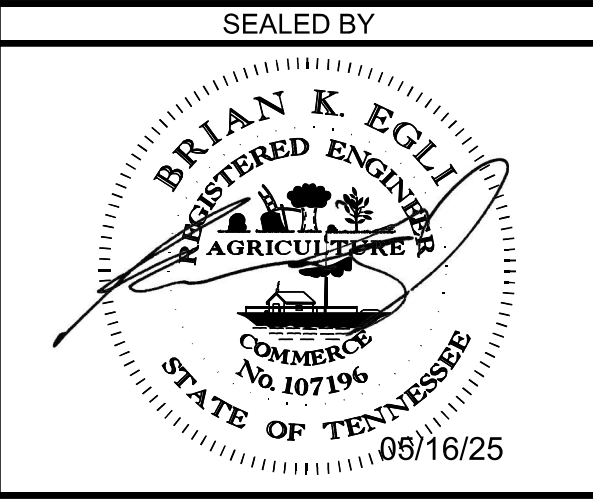
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
BRIDGE TABULATION,  
ESTIMATED QUANTITIES,  
REPAIRS NOTES AND DETAILS  
60-SR6-2.51 RT. & 60-SR6-2.48 LT.  
OVER TENNESSEE SOUTHERN  
RAILROAD AND BIG BIGBY CREEK  
60-SR6-5.85 RT. AND LT.  
OVER BIG BIGBY CREEK  
MAURY COUNTY  
2025



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60S006-M3-009		2025	B-3
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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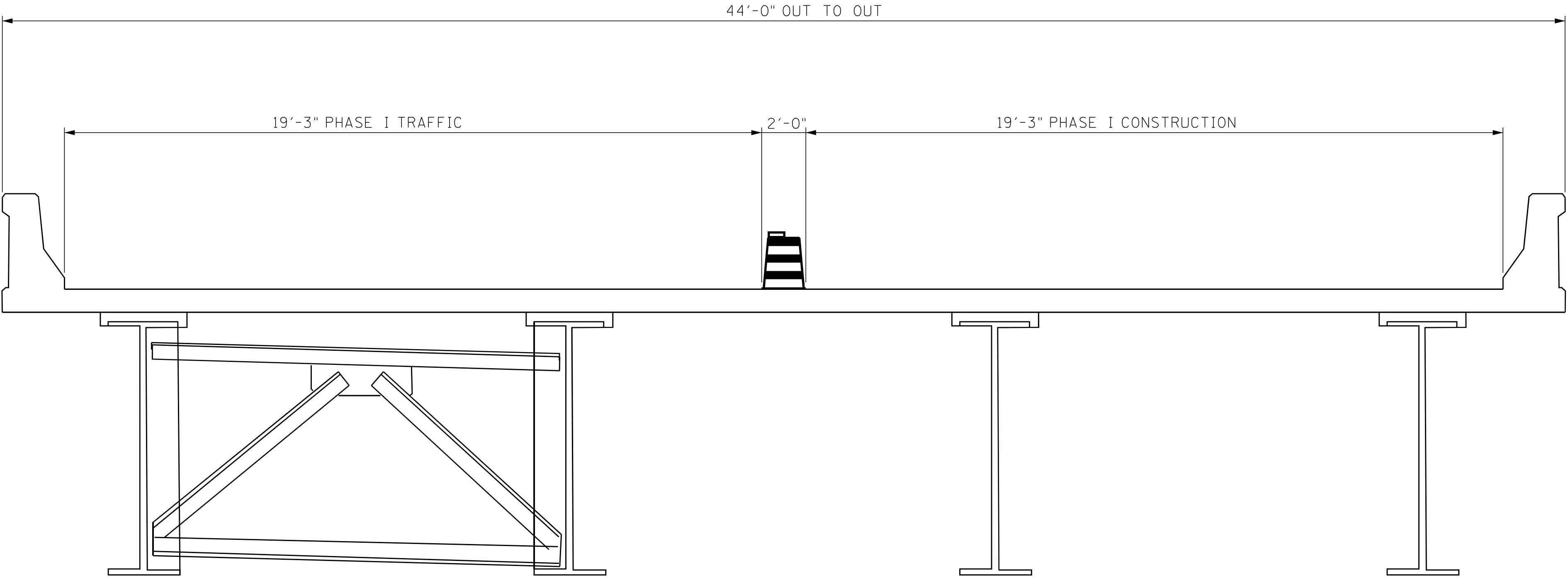
■ DENOTES: APPROXIMATE JOINT REPAIRS LOCATIONS.  
▨ DENOTES: APPROXIMATE PARTIAL DEPTHS REPAIRS LOCATIONS.



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
PLAN VIEW  
REPAIRS LOCATIONS  
60-SR6-2.51 RT. & 60-SR6-2.48 LT.  
OVER TENNESSEE SOUTHERN  
RAILROAD AND BIG BIGBY CREEK  
MAURY COUNTY  
2025

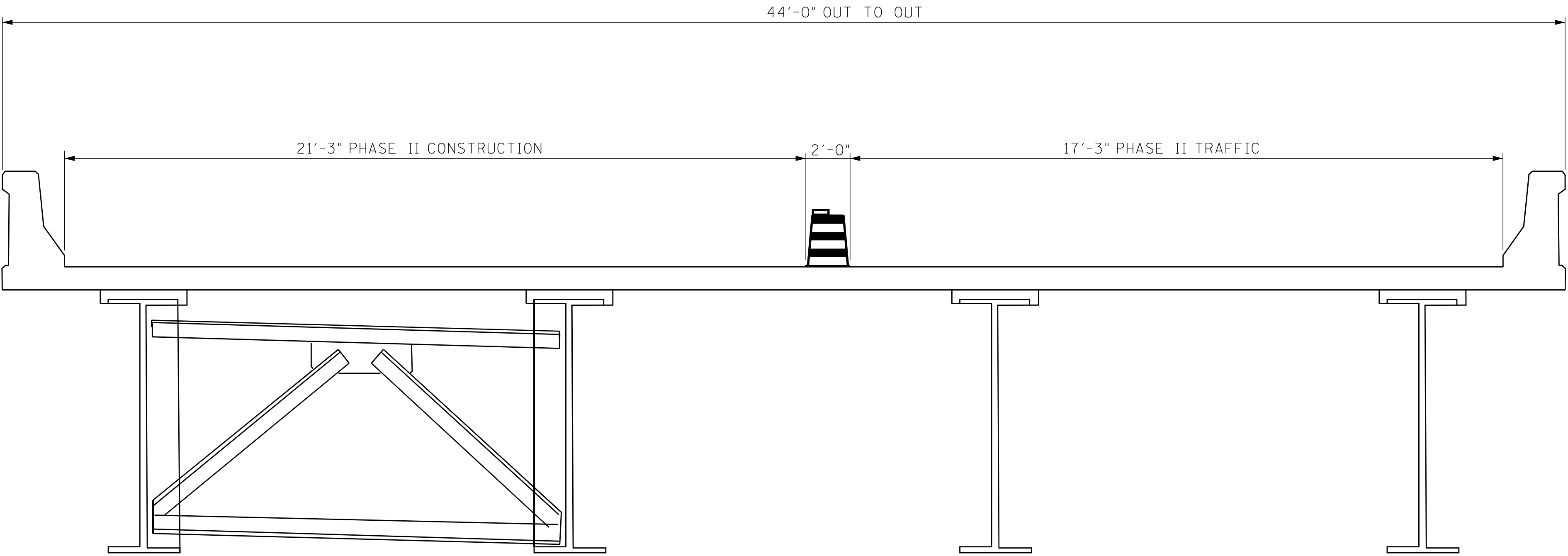
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DRAWN BY:	SILESHI ERGICHO	DATE:	11/24
SUPERVISED BY:	KEVIN MARTINKO	DATE:	11/24
CHECKED BY:		DATE:	/ /

PROJECT NO.		YEAR		SHEET NO.	
60S006-M3-009		2025		B-4	
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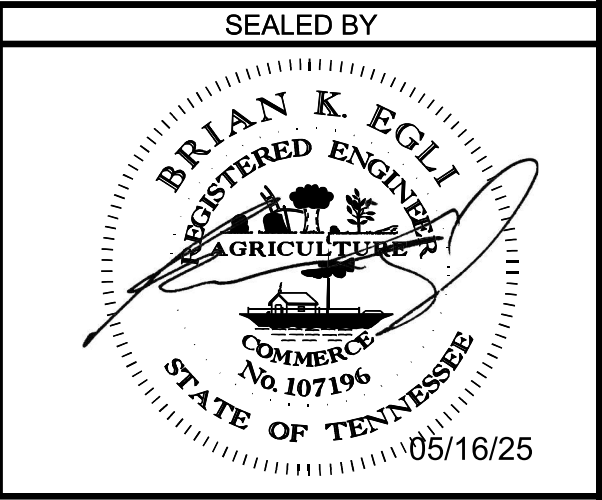
PHASE I CONSTRUCTION

(LEFT LANE - LOOKING BACK ON THE SURVEY)  
(RIGHT LANE - LOOKING AHEAD ON THE SURVEY)



PHASE II CONSTRUCTION

(LEFT LANE - LOOKING BACK ON THE SURVEY)  
(RIGHT LANE - LOOKING AHEAD ON THE SURVEY)

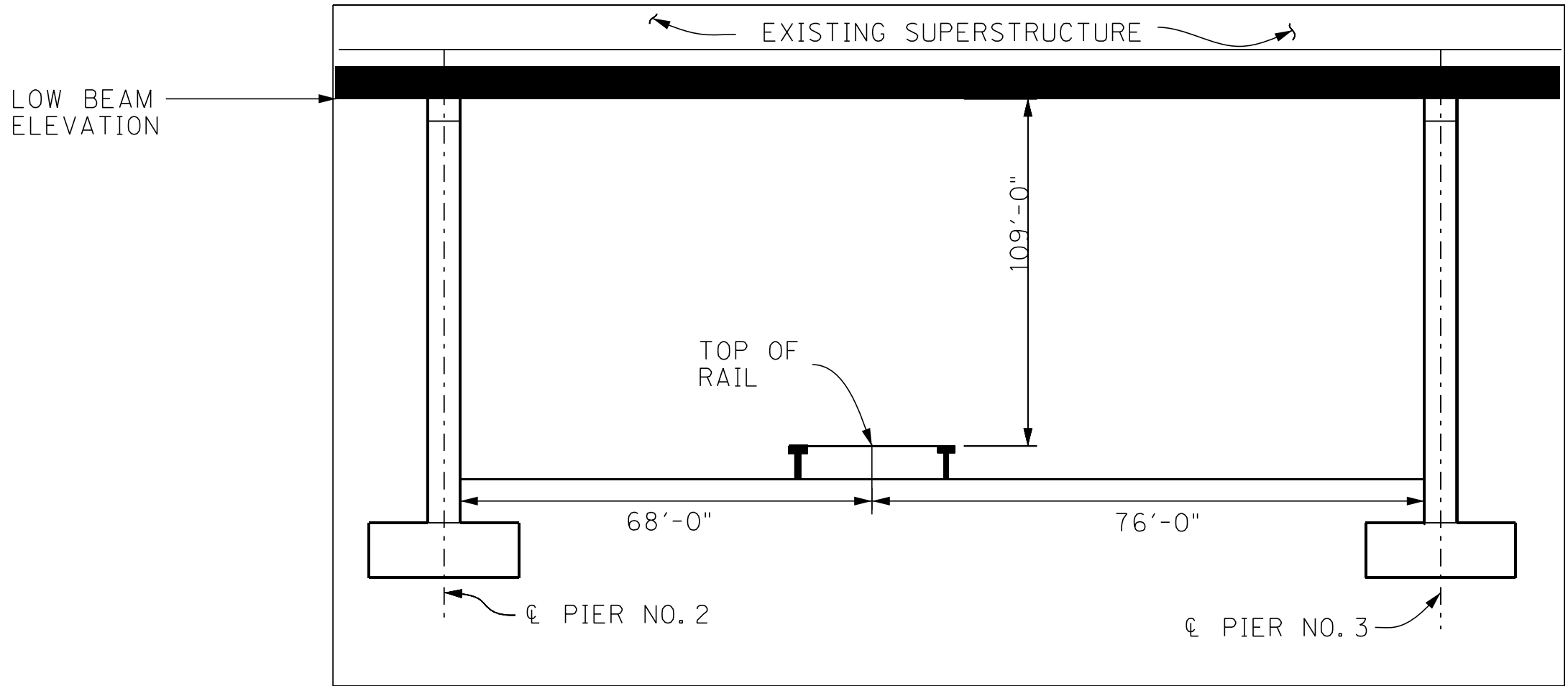


STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
PHASE CONSTRUCTION  
60-SR6-2.51 RT. & 60-SR6-2.48 LT.  
OVER TENNESSEE SOUTHERN  
RAILROAD AND BIG BIGBY CREEK  
MAURY COUNTY  
2025

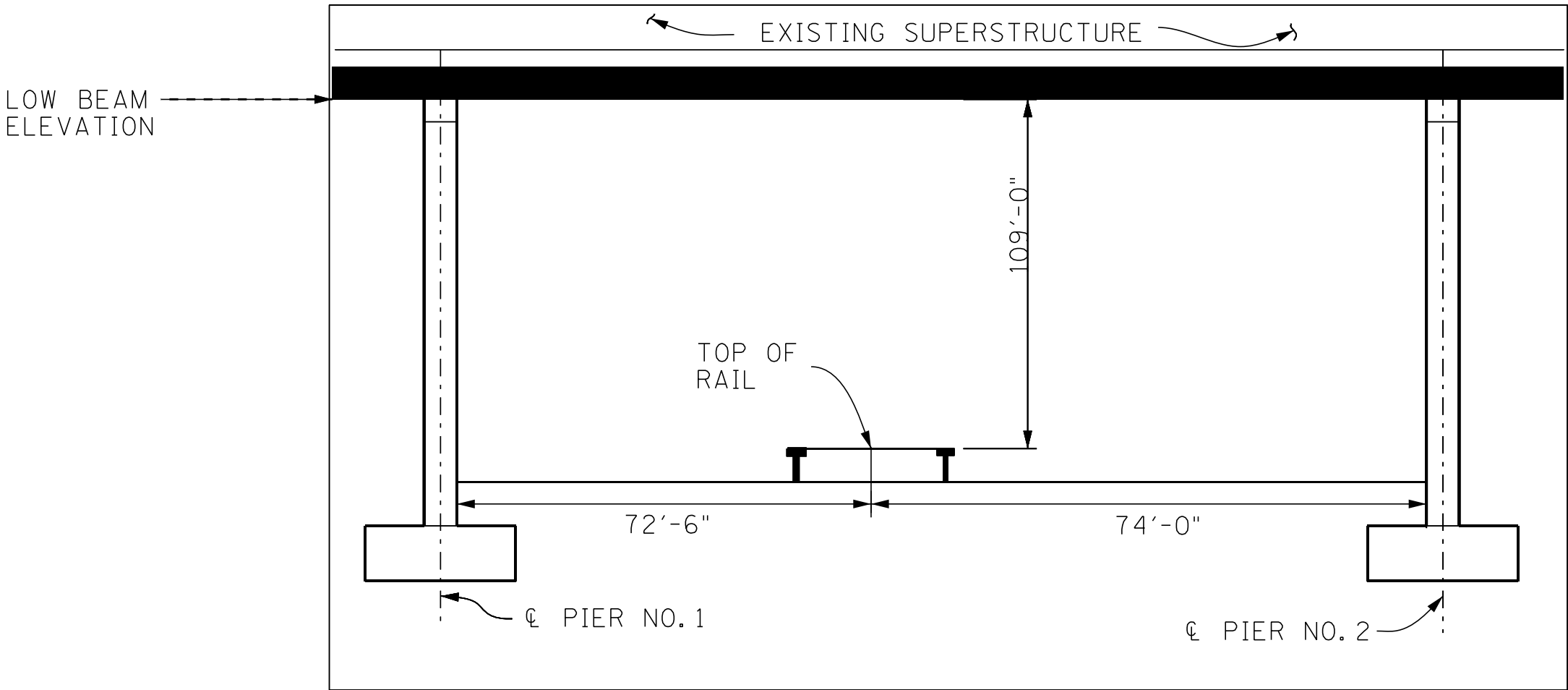
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DRAWN BY:	SILESHI ERGICHO	DATE:	11/24
SUPERVISED BY:	KEVIN MARTINKO	DATE:	11/24
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PROJECT NO.		YEAR	SHEET NO.
60S006-M3-009		2025	B-5
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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TENNESSEE SOUTHERN RAILROADR  
LEFT LANE BRIDGE.



TENNESSEE SOUTHERN RAILROADR  
RIGHT LANE BRIDGE.

SPECIAL RAILROAD NOTES

1.) THE CONTRACTOR SHALL CONDUCT HIS WORK SO AS TO PROTECT THE RAILROAD'S TRACKS AND PROPERTIES FROM ANY DAMAGE.THE WORK SHALL BE DONE IN ACCORDANCE WITH REGULATIONS STIPULATED BY TENNESSEE SOUTHERN RAILROADR ON THIS PROJECT SO AS TO MAINTAIN CLEARANCE AND NOT INTERRUPT TRAIN TRAFFIC IN ANY MANNER.

2.) THE CONTRACTOR SHALL BE REQUIRED TO SUBMIT A DETAILED AND COMPREHENSIVE SCHEDULE,PLAN AND PROCEDURE FOR REVIEW AND APPROVAL BY TENNESSEE SOUTHERN RAILROADR FOR THE FOLLOWING TASKS IN THE RAILROAD'S RIGHT-OF-WAY:

- EXPANSION JOINT REPAIRS AT APPROACH PAVEMENT ENDS.
- PARTIAL DEPTH REPAIRS BRIDGE DECK
- PARTIAL DEPTH OF APPROACH PAVEMENT
- TYPE 1 THIN EPOXY OVERLAY (EPOXY URETHANE)

3.)FOR CONCRETE DECK REPAIR OVER THE RAILROAD,DURING DEMOLITION OF THE DECK,A PROTECTION SHIELD SHALL BE ERECTED FROM THE UNDERSIDE OF THE BRIDGE OVER THE TRACK AREA TO CATCH FALLING DEBRIS.THE PROTECTION SHIELD SHALL BE SUPPORTED FROM GIRDERS OR BEAMS.THE PROTECTION SHIELD SHALL BE DESIGNED,WITH SUPPORTING CALCULATIONS,FOR A MINIMUM OF 50 POUNDS PER SQUARE FOOT PLUS THE WEIGHT OF THE EQUIPMENT,DEBRIS,PERSONNEL,AND OTHER LOADS TO BE CARRIED.

LARGE PIECES OF THE DECK SHALL NOT BE ALLOWED TO FALL ON THE PROTECTION SHIELD

A BALLAST PROTECTION SYSTEM CONSISTING OF GEOFABRIC OR CANVAS SHALL BE PLACED OVER THE TRACK STRUCTURE TO KEEP THE BALLAST CLEAN.THE SYSTEM SHALL EXTEND ALONG THE TRACK STRUCTURE FOR A MINIMUM OF 25'-0" BEYOND THE LIMITS OF THE DEMOLITION WORK,OR FARTHER IF REQUIRED BY TSRR'S CONSTRUCTION ENGINEERING AND INSPECTION REPRESENTATIVE.

THE CONTRACTOR SHALL SUBMIT DETAILED PLANS TO TENNESSEE SOUTHERN RAILROADRRAILROAD ,WITH SUPPORTING CALCULATIONS,OF THE PROTECTION SHIELD AND BALLAST PROTECTION SYSTEMS FOR APPROVAL PRIOR TO THE START OF DEMOLITION.

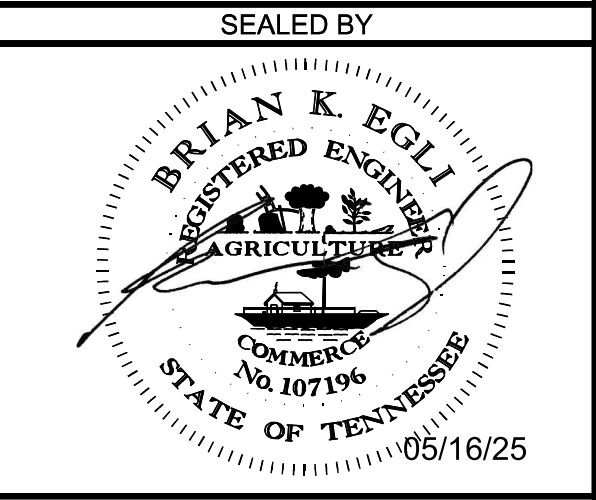
COST OF REMOVING AND DISPOSING OF DEBRIS,COMPLETE AND FULL PROTECTION OF THE CONTRACTOR WORK AREA FOR DEMOLITION SHALL BE INCLUDED IN ITEMS BID ON.

4.)NO TEMPORARY FALSEWORK IF APPLICABLE WILL BE ALLOWED THAT INFRINGES ON EXISTING HORIZONTAL AND VERTICAL CLEARANCES (SEE THIS SHEET)WITHOUT PRIOR APPROVAL OF TENNESSEE SOUTHERN RAILROAD. IT IS ANTICIPATED THAT FULL TIME FLAGGING WILL ONLY BE REQUIRED FOR FULL DEPTH CONCRETE DECK REPAIRS,AND ANY OTHER FLAGGING SERVICES REQUIREMENTS FOR THE BRIDGE DECK SEAL REPAIR CONSTRUCTION WILL BE AT THE DISCRETION OF THE RAILROAD.

5.)SEE RAILROAD AGREEMENT/STATE CONTRACT SPECIAL PROVISION 105C(R)[RAILROAD SPECIAL PROVISIONS] AND TENNESSEE SOUTHERN RAILROAD PUBLIC PROJECTS MANUAL CURRENT EDITION TO SET UP FLAGGING SERVICES,TO SUBMIT FOR INSURANCE REQUIRED FOR WORK ON HIGHWAY BRIDGE OVER THE RAILROAD,AND FOR INFORMATION ON ALL OTHER RAILROAD RULES AND SPECIFICATIONS THAT APPLY TO THIS PROJECT.

RAILROAD CONTACT ADDRESSES

TERRY J. FRANK  
DIRECTOR OF ENGINEERING  
XORAIL, INC. (REPERSENTING TENNESSEE SOUTHERN RR)  
5011 GATE PARKWAY : BUILDING 100, SUITE 400  
JACKSONVILLE, FL 32256  
OFFICE: (904) 596-1213  
FAX: (904) 443-0089  
:EMAIL: tfrank@wabtec.com

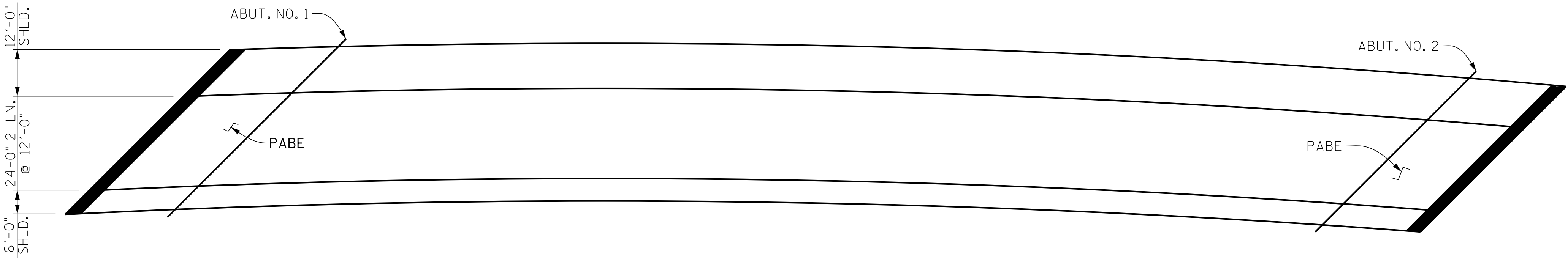


STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
VERTICAL CLEARANCES AND  
RAILROAD NOTES  
60-SR6-2.51 RT. & 60-SR6-2.48 LT.  
OVER TENNESSEE SOUTHERN  
RAILROAD AND BIG BIGBY CREEK  
MAURY COUNTY  
2025

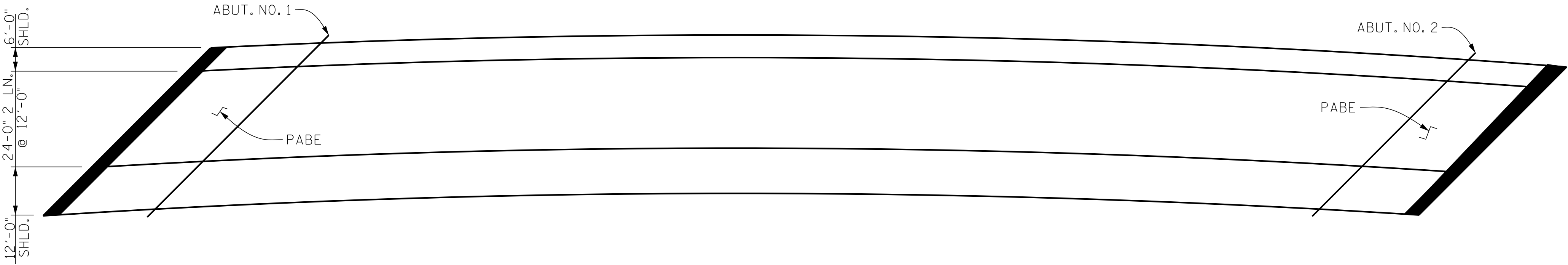
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DRAWN BY:	SILESHERGICHO	DATE:	11/24
SUPERVISED BY:	KEVIN MARTINKO	DATE:	/ /
CHECKED BY:		DATE:	/ /



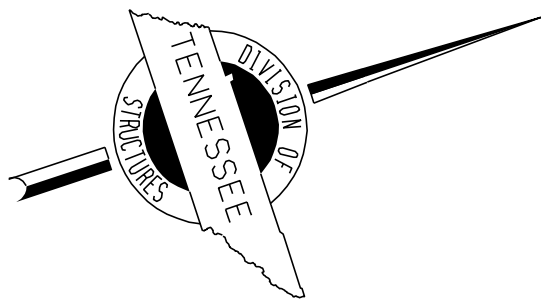
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60S006-M3-009		2025	B-6
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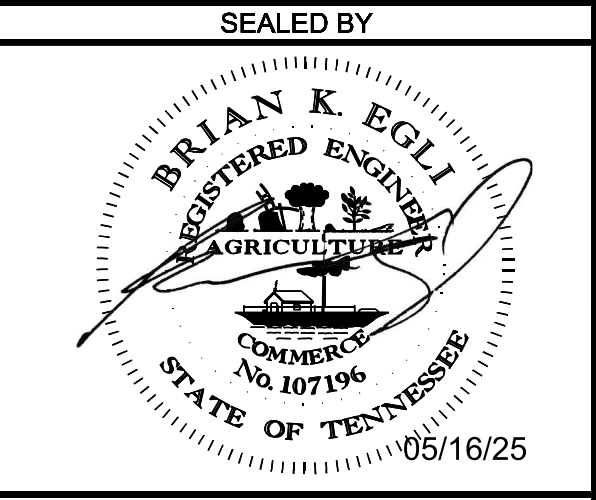
PLAN VIEW  
60-SR6-5.58 LT.



PLAN VIEW  
60-SR6-5.58 RT.



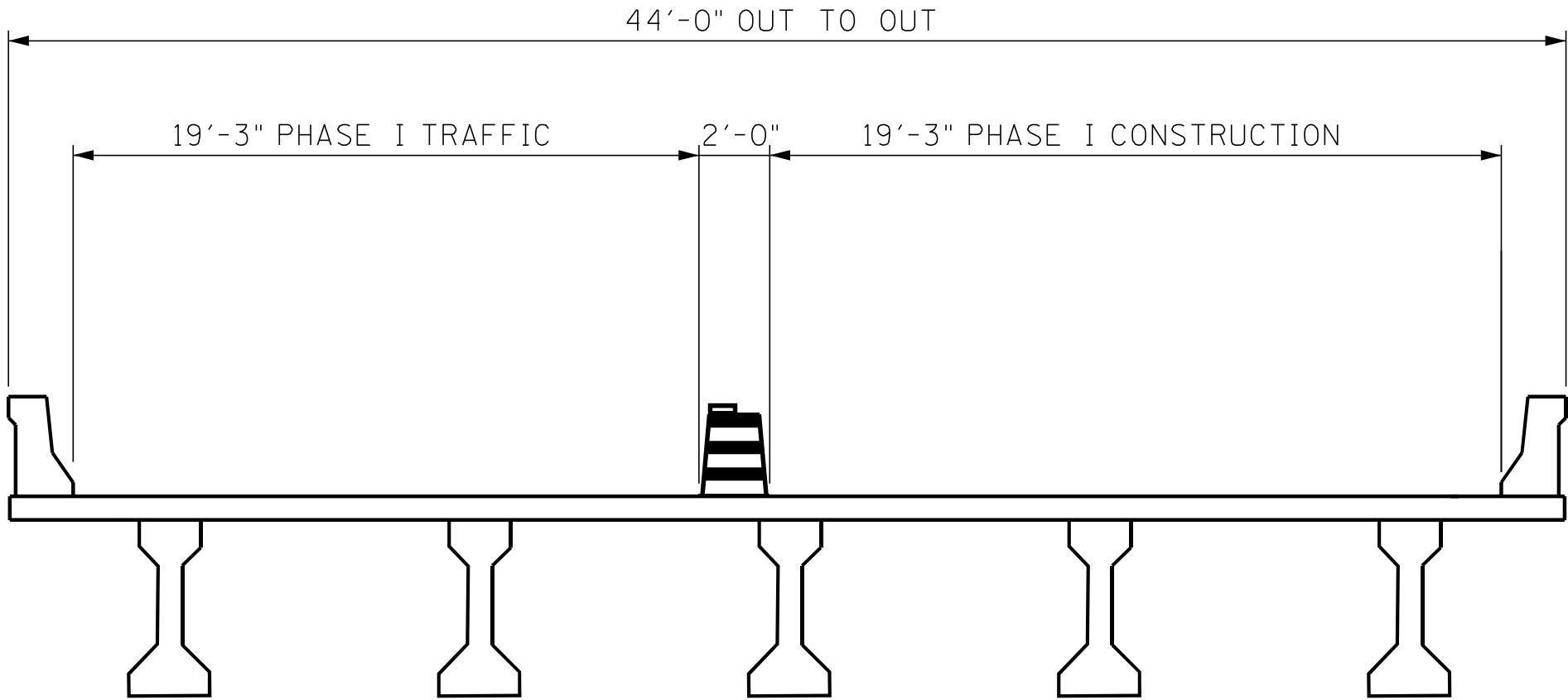
■ DENOTES: APPROXIMATE JOINT REPAIRS LOCATIONS.



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
PLAN VIEW  
REPAIRS LOCATIONS  
60-SR6-5.58 RT. & LT.  
OVER  
BIG BIGBY CREEK  
MAURY COUNTY  
2025

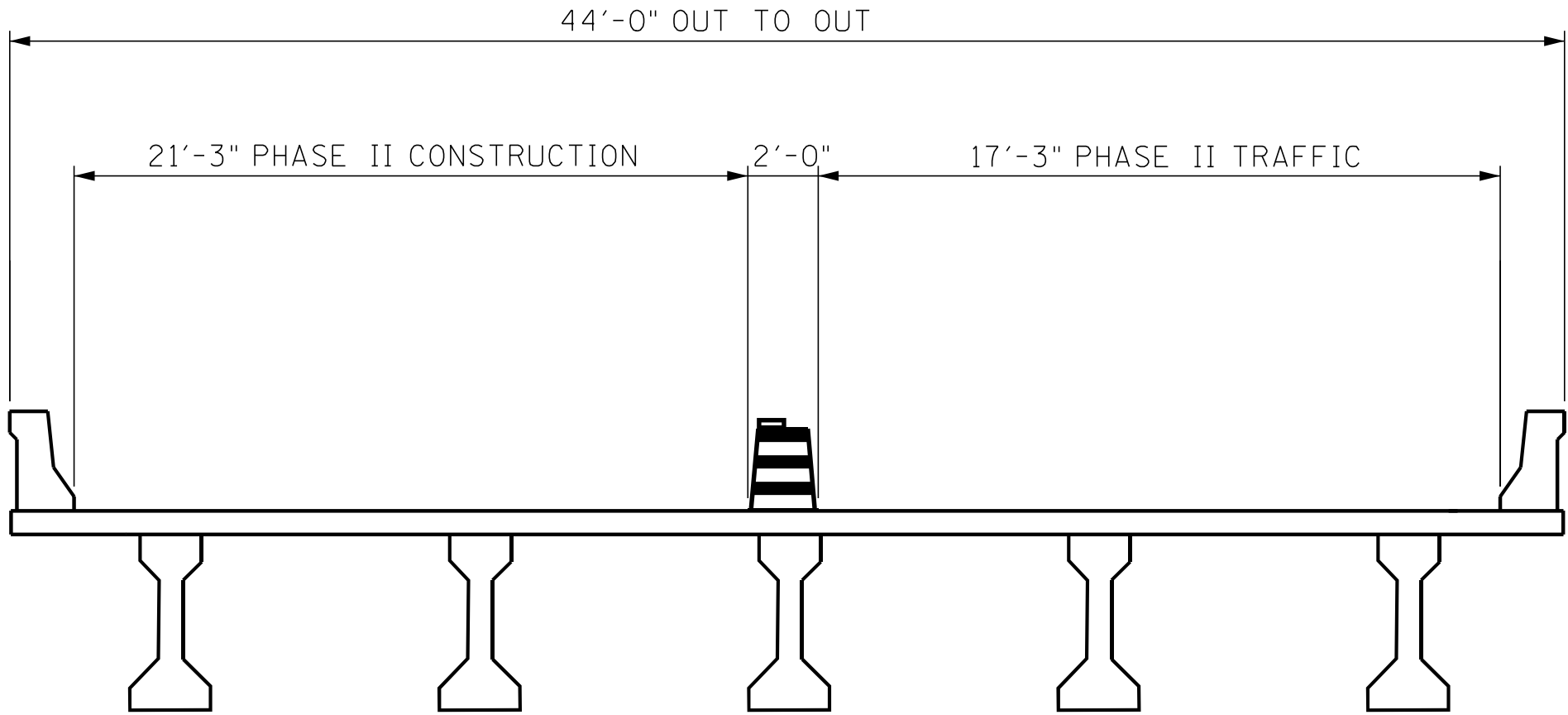
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DRAWN BY: KEVIN MARTINKO  
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CHECKED BY:   
DATE: 11/24  
DATE: 11/24  
DATE: 11/24

PROJECT NO.		YEAR	SHEET NO.
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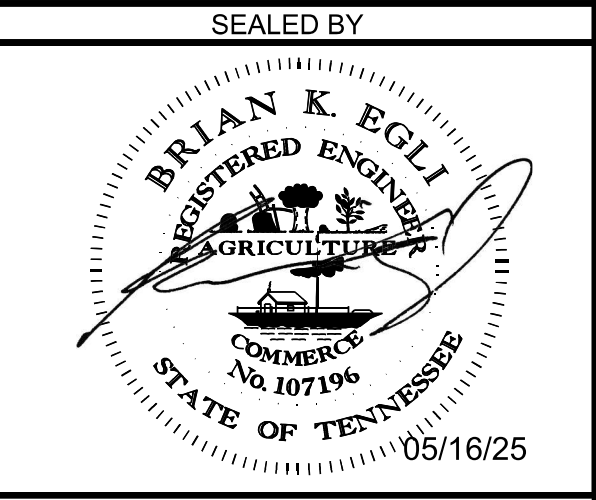
PHASE I CONSTRUCTION

(LEFT LANE - LOOKING BACK ON THE SURVEY)  
(RIGHT LANE - LOOKING AHEAD ON THE SURVEY)



PHASE II CONSTRUCTION

(LEFT LANE - LOOKING BACK ON THE SURVEY)  
(RIGHT LANE - LOOKING AHEAD ON THE SURVEY)



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
PHASE CONSTRUCTION  
60-SR6-5.58 RT. & LT.  
OVER  
BIG BIGBY CREEK  
MAURY COUNTY  
2025

PIN NO.:	122522.00	DATE:	/ /
DESIGN BY:		DATE:	11/24
DRAWN BY:	SILESHI ERGICHO	DATE:	11/24
SUPERVISED BY:	KEVIN MARTINKO	DATE:	/ /
CHECKED BY:		DATE:	/ /



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\$\$\$\$DGN\$PEC\$\$\$\$\$\$\$\$

PIN NO.:	122522.00	DATE:	/ /
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TYPE 1 THIN EPOXY OVERLAY NOTES :

TYPE 1 THIN EPOXY OVERLAY SYSTEM - USE DECK PRETREATMENT/PRIMER PER MANUFACTURER'S RECOMMENDATION, AND 2 LIFTS OF AN EPOXY-URETHANE COPOLYMER AND AGGREGATE. TYPE 1 OVERLAY SHALL BE APPLIED MECHANICALLY USING METERED EQUIPMENT; HAND MIXING OF MATERIAL IS NOT PERMITTED.

THIN OVERLAY SYSTEM SHALL BE FROM THE QUALIFIED PRODUCTS LIST 23.005 TYPE 1 THIN OVERLAY (EPOXY URETHANE). MINIMUM OVERLAY THICKNESS SHALL BE 3/8 INCH.

APPLICATION EQUIPMENT SHOULD :

A) BE CAPABLE OF METERING, MIXING AND DISTRIBUTING THE POLYMER AND PRETREATMENT TO MANUFACTURER'S RECOMMENDATION.

B) USE AN APPLICATION MACHINE THAT FEATURES POSITIVE DISPLACEMENT VOLUMETRIC METERING PUMPS CONTROLLED BY A HYDRAULIC POWER UNIT.

C) STORE COMPONENTS IN TEMPERATURE CONTROLLED RESERVOIRS CAPABLE OF MAINTAINING 100 DEGREES FAHRENHEIT (PLUS OR MINUS 10 DEGREES) TO INSURE OPTIMAL MIXING.

D) CHECK MIXING RATIO AT THE PUMP OUTLETS AS WELL AS CYCLE COUNTING CAPABILITIES TO MONITOR OUTPUT ON STANDARD FEATURES.

E) USE MOTIONLESS IN-LINE MIXING SO AS TO NOT OVERLY SHEAR THE MATERIAL TO ENTRAP AIR IN THE MIX.

F) MAXIMIZE MATERIAL WORKING TIME BY MIXING IT IMMEDIATELY BEFORE DISPENSING.

AGGREGATE SHALL BE ANGULAR, HAVING LESS THAN 0.2% MOISTURE AND FREE OF DIRT, CLAY, ASPHALT AND OTHER FOREIGN OR ORGANIC MATERIALS. AGGREGATE FOR ALL LAYERS SHALL BE BAUXITE OR FLINT ROCK PRODUCTS FLINT AND MEETS THE FOLLOWING GRADATION:

SIEVE SIZE	% PASSING
NO. 6	95-100
NO. 10	10-35
NO. 20	0-3

FULL AND PARTIAL DEPTH DECK REPAIR SHALL CURE A MINIMUM OF 28 DAYS BEFORE THE OVERLAY IS PLACED. THE 28 DAYS MAY BE WAIVED IF THE OVERLAY MANUFACTURER PROVIDES A METHOD OF TESTING THE REPAIRED AREAS AND APPROVES THE PLACEMENT BY LETTER. TRAFFIC SHALL BE ALLOWED TO USE THE BRIDGE DURING THE CURING PERIOD OF THE PATCHES BUT NOT AFTER SHOTBLASTING. MAGNESIUM PHOSPHATE BASED MATERIALS WILL NOT BE ALLOWED.

THE CONCRETE DECK SURFACE SHALL BE CLEANED BY SHOTBLASTING TO REMOVE ANY OIL, DIRT, RUBBER, TRAFFIC STRIPING, OR ANY OTHER POTENTIAL DETRIMENTAL MATERIAL SUCH AS CURING COMPOUND AND LAITANCES, WHICH THE MANUFACTURER AND ENGINEER'S OPINION WOULD PREVENT PROPER BONDING AND CURING OF THE MATERIAL. IN AREAS WHERE SHOTBLASTING EQUIPMENT CAN NOT REACH (I.E., ALONG CURBS AND BRIDGE RAILS) SANDBLASTING IS PERMITTED TO AN EXTENT TO THE ENGINEER'S AND MANUFACTURER'S APPROVAL. IMMEDIATELY BEFORE APPLICATION, ALL PREPARED SURFACES SHALL BE CLEANED WITH COMPRESSED AIR OR VACUUMED TO REMOVE DUST AND DEBRIS. THE CONTRACTOR IS TO PREVENT THE TRACKING OF TACK COAT AND CONSTRUCTION DEBRIS ACROSS THE BRIDGE DECK PRIOR TO THE APPLICATION OF THE THIN OVERLAY. MILLING THE BRIDGE DECK WILL NOT BE AN OPTION FOR TACK COAT OR DEBRIS REMOVAL. REMOVAL SHALL BE AT THE CONTRACTOR'S EXPENSE.

ALL SURFACES THAT ARE TREATED SHALL BE DRY AT THE TIME OF APPLICATION. THE OVERLAY SHALL NOT BE APPLIED WHEN IT HAS RAINED 24 HOURS PRIOR TO, OR RAIN IS FORECAST WITHIN 8 HOURS AFTER, APPLICATION. THE MOISTURE CONTENT IN THE DECK SUBSTRATE SHALL BE TESTED. MOISTURE IS NOT TO EXCEED 4.5 PERCENT WHEN MEASURED BY ELECTRONIC METER. IF THE TEST SHOWS EXCESS MOISTURE, THE DECK SHALL CONTINUE TO DRY BEFORE APPLICATION PROCEEDS.

BLUSHING (A WAXY SURFACE COATING ON THE EPOXY) IS CAUSED BY THE REACTION OF MOISTURE WITH THE HARDENING AGENT. BLUSHING CREATES A SURFACE THAT MAKES FUTURE LAYERS DIFFICULT TO ADHERE. LIFTS THAT SHOW SIGNS OF BLUSHING SHALL BE REMOVED AND REPLACED PRIOR TO APPLICATION OF THE NEXT. THE COST TO REMOVE AND REPLACE THESE AREAS SHALL BE AT THE CONTRACTOR'S EXPENSE.

TRAFFIC, OTHER THAN APPLICATION EQUIPMENT, SHALL NOT BE ALLOWED ON ANY PORTION OF THE DECK THAT HAS BEEN SHOTBLASTED OR WHERE PART OF THE APPLICATION HAS BEEN PLACED.

SEE MANUFACTURER'S RECOMMENDATIONS FOR REQUIRED AMBIENT AND SURFACE TEMPERATURES AND HUMIDITY LIMITS FOR APPLICATION.

THE MANUFACTURER SHALL HAVE A REPRESENTATIVE ON THE JOB SITE AT ALL TIMES DURING APPLICATION AND CURE TIME. THE REPRESENTATIVE, ALONG WITH CONSULTATION WITH ENGINEER, MAY SUSPEND ANY ITEM OF WORK THAT IS SUSPECT AND DOES NOT MEET THE REQUIREMENTS OF THE SPECIFICATIONS. WORK SHALL NOT RESUME UNTIL THE ENGINEER AND REPRESENTATIVE ARE SATISFIED THAT APPROPRIATE REMEDIAL ACTION HAS BEEN TAKEN BY THE CONTRACTOR.

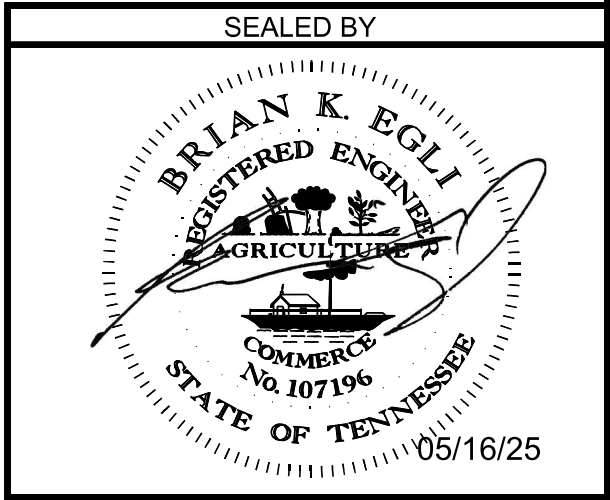
ALL COSTS FOR AGGREGATE, EPOXY FOR MINIMUM OF TWO LIFTS, SURFACE PREPARATION, LABOR AND ANY OTHER MISCELLANEOUS MATERIALS REQUIRED TO PLACE THIN OVERLAY SHALL BE INCLUDED IN ITEM NO. 617-04.01, TYPE 1 THIN EPOXY OVERLAY (EPOXY URETHANE), S.Y.

THICKNESS VERIFICATION: THE PROJECT ENGINEER SHALL BE NOTIFIED OF THE NUMBER OF GALLONS USED ON THE PROJECT WITH NOTARIZED QUANTITY STATEMENTS FROM THE CONTRACTOR AND THE MANUFACTURER. THE CONTRACTOR SHALL VERIFY TO TDOT THAT THE OVERLAY IS AN AVERAGE OF AT LEAST 3/8 INCH THICK AT THREE RANDOM LOCATIONS AGREED UPON BY THE PROJECT ENGINEER AND THE MATERIAL MANUFACTURER REPRESENTATIVE. IF 3/8 INCH AVERAGE IS NOT ACHIEVED, A RETEST SHALL BE PERFORMED IN ADJOINING AREAS. THIN AREAS SHALL BE RE-COATED AS DESCRIBED ABOVE BY THE CONTRACTOR AND RE-VERIFIED AT NO ADDITIONAL COST TO TDOT. THIS VERIFICATION MAY CONSIST OF CORES MADE BY THE CONTRACTOR WITH A CORING BIT NOT LESS THAN 1½" DIAMETER. THE TESTED AREAS SHALL BE REPAIRED BY THE CONTRACTOR BEFORE FINAL ACCEPTANCE BY THE PROJECT ENGINEER.

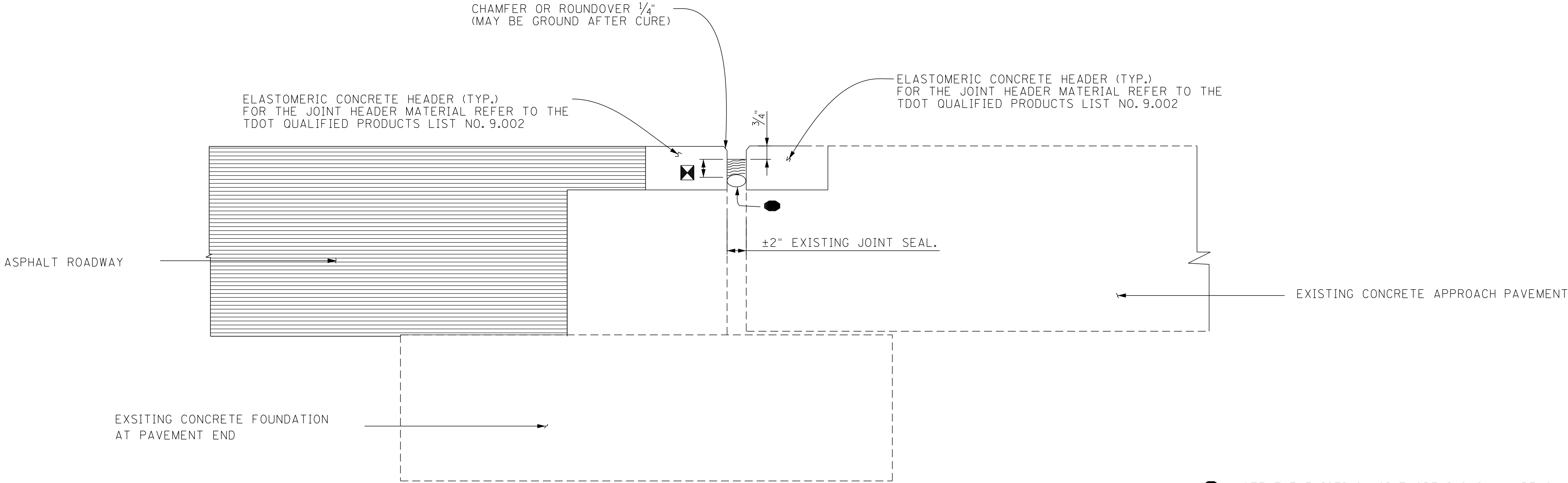
THE MANUFACTURER SHALL HAVE A REP ON THE JOB SITE AT ALL TIME DURING APPLICATION AND CURE TIME. THE REP WITH THE ENGINEER, MAY SUSPEND AN ITEM OF WORK THAT IS SUSPECT AND DOES NOT MEET THE REQUIREMENTS OF THE SPECS. WORK SHALL NOT RESUME UNTIL THE ENGINEER AND REP ARE SATISFIED THAT APPROPRIATE REMEDIAL ACTION HAS BEEN TAKEN BY THE CONTRACTOR.

PROJECT NO.		YEAR		SHEET NO.	
60S006-M3-009		2025		B-8	
REVISIONS					
NO.	DATE	BY	BRIEF DESCRIPTION		
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	- -				
	- -				
	- -				
	- -				

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
TYPE 1 THIN EPOXY  
OVERLAY NOTES  
60-SR6-2.51 RT. & 60-SR6-2.48 LT.  
OVER TENNESSEE SOUTHERN  
RAILROAD AND BIG BIGBY CREEK  
MAURY COUNTY  
2025



PROJECT NO.		YEAR	SHEET NO.
60S006-M3-009		2025	B-9
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
	- -		
	- -		
	- -		
	- -		
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EXPANSION JOINT SYSTEM

- NOTE: THE EXISTING JOINT OPENING SHALL BE CAULKED WITH A BACKER ROD OF SUITABLE DIAMETER,THE ROD SHALL BE PLACED AT A DEPTH TO INSURE THE CORRECT WIDTH/DEPTH RATIO OF THE NEW JOINT SEALANT MATERIAL,BACKER ROD SHALL BE AS PER JOINT MANUFACTURERS RECOMMENDATIONS.
- NOTE: FULL DEPTH OF ALL EXISTING JOINTS SHALL SHALL BE RESEALED WITH NEW JOINT SEALER. THE NEW JOINT SEALER SHALL BE A COLD PUOR TWO PART SILICONE SEALER FROM QPL 5.001.

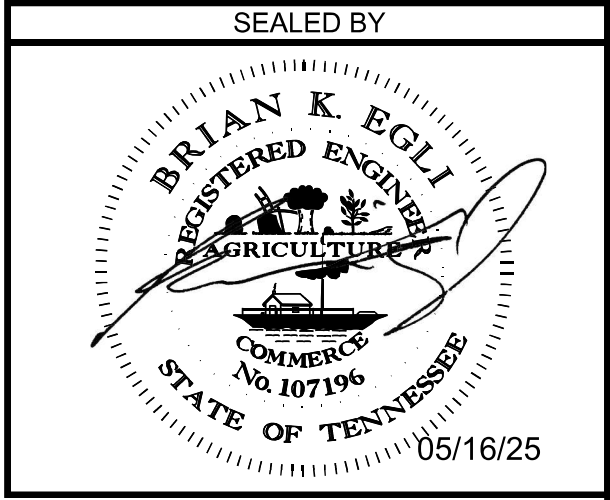
EXPANSION JOINT REPAIR NOTES:

THE JOINT HEADER SYSTEM SHALL BE FROM QPL 9.002. THE JOINT SEAL SYSTEM CONSISTS OF THE ELASTOMERIC HEADER MATERIAL AND A TWO-PART COLD POUR SILICONE SEALANT. THE CONTRACTOR SHALL ALSO HAVE THE OPTION OF USING A PRECOMPRESSED FOAM WITH SILICONE TOPPED PRODUCT (SIMILAR TO AND INCLUDING BEJS) FROM QPL 7.001 WITH AN APPROPRIATELY SIZED SEAL FOR THE JOINT OPENING,AND AN ELASTOMERIC CONCRETE FROM QPL 9.001.THE SYSTEM SHALL BE INSTALLED UNDER THE DIRECT SUPERVISION OF AN AUTHORIZED TECHNICIAN PROVIDED BY THE SYSTEM MANUFACTURER. THE TECHNICIAN MUST APPROVE ALL ASPECTS OF THE GEOMETRY AND PREPARATION OF THE JOINT LOCATIONS PRIOR TO ANY INSTALLATION OF THE JOINT SYSTEM MATERIALS. PRODUCTS FROM QPL 7.001 "ON A ROLL" AND "COMPRESSION ONLY" WILL NOT BE ALLOWED. THE TOP OF THE QPL 7.001 JOINT FILLER SHALL BE A MINIMUM OF 3/4 OF AN INCH BELOW THE ROADWAY SURFACE UNLESS THE MANUFACTURER REQUIRES A DEEPER DEPTH.

MANUFACTURERS SPECIFICATIONS AND INSTALLATION PROCEDURES SHALL BE SUBMITTED TO THE PROJECT ENGINEER FOR REVIEW PRIOR TO THE JOINT REPLACEMENT/REPAIR WORK. THE MANUFACTURER AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE WORKMANSHIP OF THE JOINT INSTALLATION.

PRIOR TO THE INSTALLATION OF THE NEW JOINT,THE EXISTING JOINT OPENING SHALL BE CLEANED OF ALL DIRT,DEBRIS, AND PRIOR CONSTRUCTION MATERIAL,ETC.,THE FULL DEPTH OF THE OPENING. THE SURFACES WHERE THE NEW MATERIAL BONDS TO STAY IN PLACE,SHALL BE CLEANED PER MANUFACTURERS RECOMMENDATION,TO REMOVE ANY SUBSTANCES THAT WOULD INHIBIT BONDING.

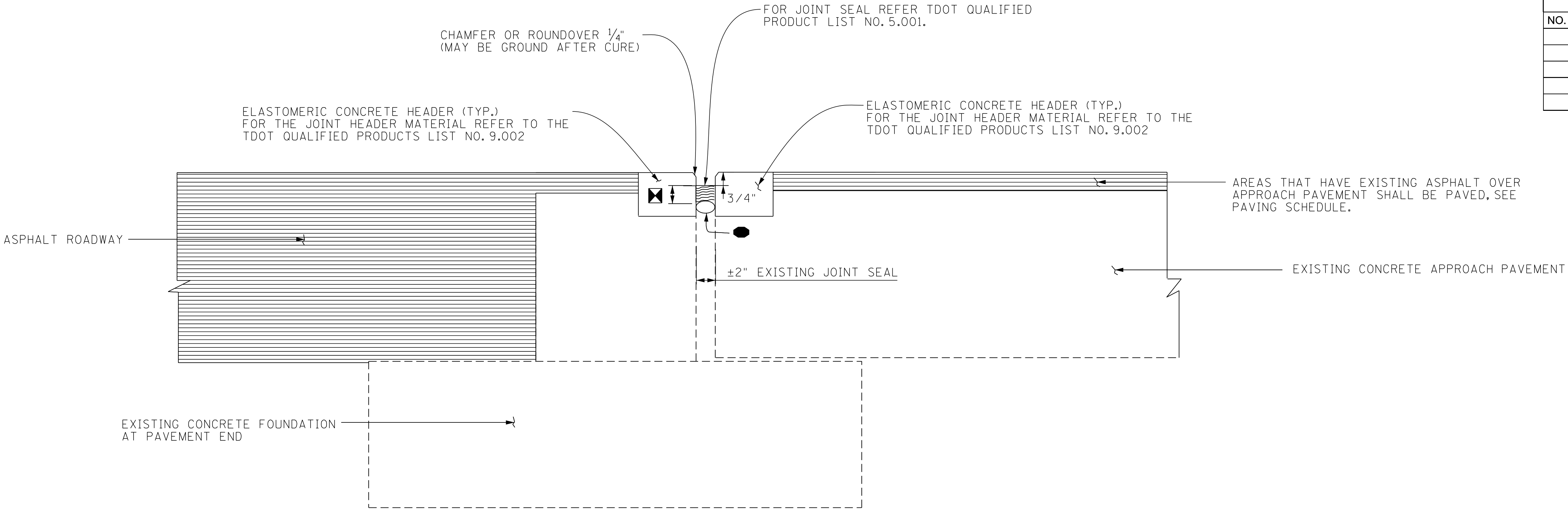
THE COST FOR REMOVING THE OLD JOINT SYSTEM,INSTALLING THE NEW JOINT SYSTEM,LABOR, AND ANY MISCELLANEOUS MATERIALS NECESSARY TO INSTALL THE NEW EXPANSION JOINT,IS TO BE INCLUDED UNDER ITEM NUMBER 604-10.44,EXPANSION JOINT REPAIRS,L.F.



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
EXPANSION JOINT  
DETAILS AT APPROACH  
PAVEMENT ENDS  
60-SR6-2.51 RT. & 60-SR6-2.48 LT.  
OVER TENNESSEE SOUTHERN  
RAILROAD AND BIG BIGBY CREEK  
MAURY COUNTY  
2025



PROJECT NO.		YEAR	SHEET NO.
60S006-M3-009		2025	B-10
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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	- -		
	- -		
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EXPANSION JOINT SYSTEM

EXPANSION JOINT REPAIR NOTES:

- NOTE: THE EXISTING JOINT OPENING SHALL BE CAULKED WITH A BACKER ROD OF SUITABLE DIAMETER, THE ROD SHALL BE PLACED AT A DEPTH TO INSURE THE CORRECT WIDTH/DEPTH RATIO OF THE NEW JOINT SEALANT MATERIAL, BACKER ROD SHALL BE AS PER JOINT MANUFACTURER'S RECOMMENDATIONS.
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AFTER PAVING AND SEALING OVER THE EXISTING EXPANSION JOINT, THE ASPHALT SHALL BE CUT OUT AS SHOWN IN THE NEW EXPANSION JOINT DETAIL.

PRIOR TO THE INSTALLATION OF THE NEW JOINT, THE EXISTING JOINT OPENING SHALL BE CLEANED OF ALL DIRT, DEBRIS, AND PRIOR CONSTRUCTION MATERIAL, ETC., THE FULL DEPTH OF THE OPENING. THE SURFACES WHERE THE NEW MATERIAL BONDS TO STAY IN PLACE, SHALL BE CLEANED PER MANUFACTURERS RECOMMENDATION, TO REMOVE ANY SUBSTANCES THAT WOULD INHIBIT BONDING.

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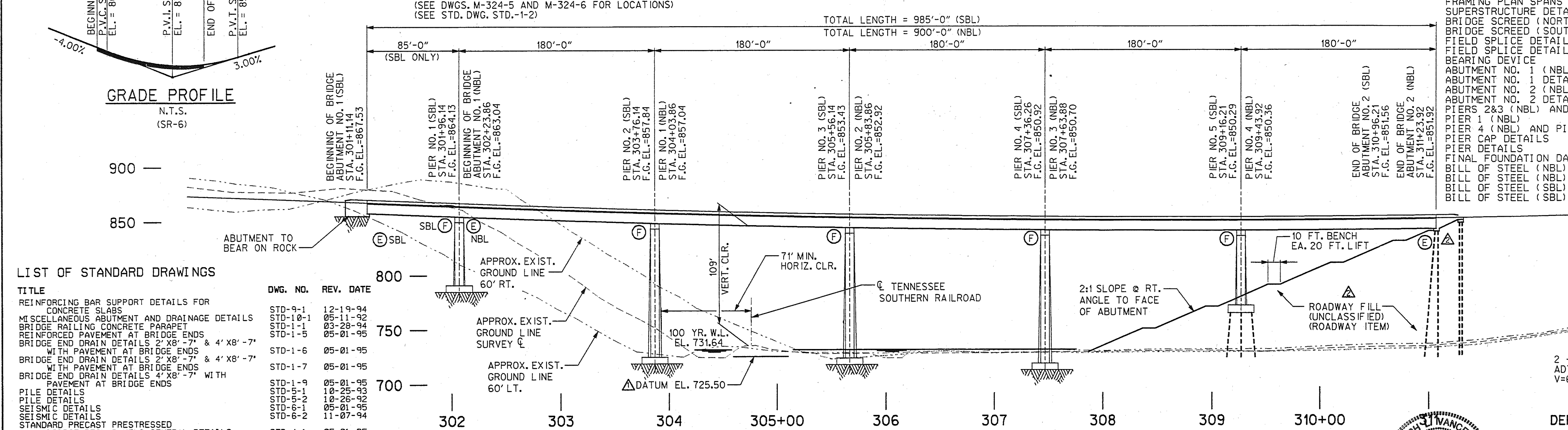
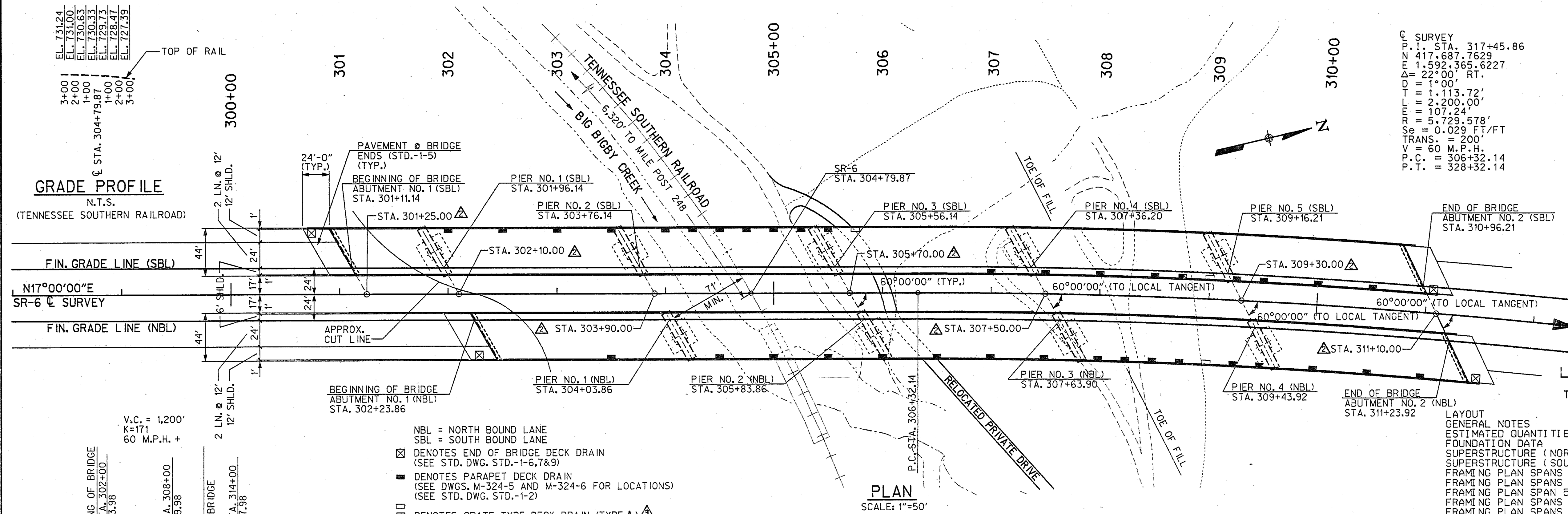
PIN NO.:	122522.00	DATE:	/ /
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DRAWN BY:	SILESHI ERGICHO	DATE:	11/24
SUPERVISED BY:	KEVIN MARTINKO	DATE:	/ /
CHECKED BY:		DATE:	/ /

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
EXPANSION JOINT  
DETAILS AT APPROACH  
PAVEMENT ENDS  
60-SR6-5.58 RT. & LT.  
OVER  
BIG BIGBY CREEK  
MAURY COUNTY  
2025



CONST. NO.		60002-3269-14	
PROJECT NO.		YEAR	SHEET NO.
NH-6 (34)		1995	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	2/01/96	JHP	DATUM ELEV. ADDED
2	6/26/96	GSH	REVISED ABUT. 2 TO PILE SUPPORT DENOTED ROADWAY FILL & STATION AND ADDED REVISION DATES
3	8/29/96	HMB	Grate Type 2 to Type 1

C. SURVEY  
 P. 1. STA. 317+45.86  
 N. 417.687.7629  
 E. 1,592.365.6227  
 D. 22°00' RT.  
 D. 1.113.72'  
 T. 2.200.00'  
 L. 107.24'  
 R. 5.729.578'  
 S. 0.029 FT/FT  
 TRANS. = 200'  
 V. = 60 M.P.H.  
 P.C. = 308+32.14  
 P.T. = 328+32.14



TITLE	DWG. NO.	REV. DATE
REINFORCING BAR SUPPORT DETAILS FOR CONCRETE SLABS	STD-9-1	12-19-94
MISCELLANEOUS ABUTMENT AND DRAINAGE DETAILS	STD-10-1	05-11-92
BRIDGE RAILING CONCRETE PARAPET	STD-1-1	03-28-94
REINFORCED PAVEMENT AT BRIDGE ENDS	STD-1-5	05-01-95
BRIDGE END DRAIN DETAILS 2' X 8' - 7' & 4' X 8' - 7' WITH PAVEMENT AT BRIDGE ENDS	STD-1-6	05-01-95
BRIDGE END DRAIN DETAILS 2' X 8' - 7' & 4' X 8' - 7' WITH PAVEMENT AT BRIDGE ENDS	STD-1-7	05-01-95
BRIDGE END DRAIN DETAILS 4' X 8' - 7' WITH PAVEMENT AT BRIDGE ENDS	STD-1-9	05-01-95
PILE DETAILS	STD-5-1	10-26-93
PILE DETAILS	STD-5-2	10-26-93
SEISMIC DETAILS	STD-6-1	05-01-95
SEISMIC DETAILS	STD-6-2	11-07-94
STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	STD-4-1	05-01-95
STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS DESIGN CRITERIA	STD-4-2	05-01-95
STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	STD-4-3	11-07-94
STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS CONSTRUCTION DETAILS	STD-4-4	11-07-94
* STEEL SLIDER PLATE ASSEMBLIES FOR CONCRETE PARAPET AND BRIDGE DECK DRAIN DETAILS	STD-1-2	09-11-95
SLOPE PROTECTION	RD-SA-1	04-29-90

TITLE	DWG. NO.	REV. DATE
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STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS DESIGN CRITERIA	STD-4-2	05-01-95
STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	STD-4-3	11-07-94
STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS CONSTRUCTION DETAILS	STD-4-4	11-07-94
* STEEL SLIDER PLATE ASSEMBLIES FOR CONCRETE PARAPET AND BRIDGE DECK DRAIN DETAILS	STD-1-2	09-11-95
SLOPE PROTECTION	RD-SA-1	04-29-90

DESIGNED BY JODY VANCE DATE 10/95  
 DRAWN BY RICHARD VERNON DATE 10/95  
 SUPERVISED BY J. M. BARRY DATE 10/95  
 CHECKED BY JODY VANCE DATE 10/95

TITLE	DWG. NO.	REV. DATE
APPROVAL OF SHOP DRAWINGS	105A	03-06-95
STEEL STRUCTURES	602	03-03-95

(E) - DENOTES EXPANSION  
 (F) - DENOTES FIXED  
 U.N.O. - DENOTES UNLESS NOTED OTHERWISE

HYDRAULIC DATA	
DRAINAGE AREA	6.54 SQ. MI.
DESIGN DISCHARGE (100 YR.)	4,409 CFS
WATER AREA PROVIDED BELOW ELEV. 731.64	1250 FT.
100 YR. VELOCITY	3.53 FPS
100 YR. BRIDGE BACKWATER	0.0 FT. @ ELEV. 736.05
ROADWAY OVERTOPPING ELEV.	850.27
500 YR. DISCHARGE	5,784 CFS @ ELEV. 736.52

LAYOUT  
 BRIDGE NO. 1  
 STATE ROUTE 6 OVER BIG BIGBY CREEK  
 AND TENNESSEE SOUTHERN RAILROAD  
 BRIDGE I.D. NO. 60SR0060083 (RL)  
 BRIDGE I.D. NO. 60SR0060084 (LL)  
 STATION 306+00  
 MAURY COUNTY  
 1995

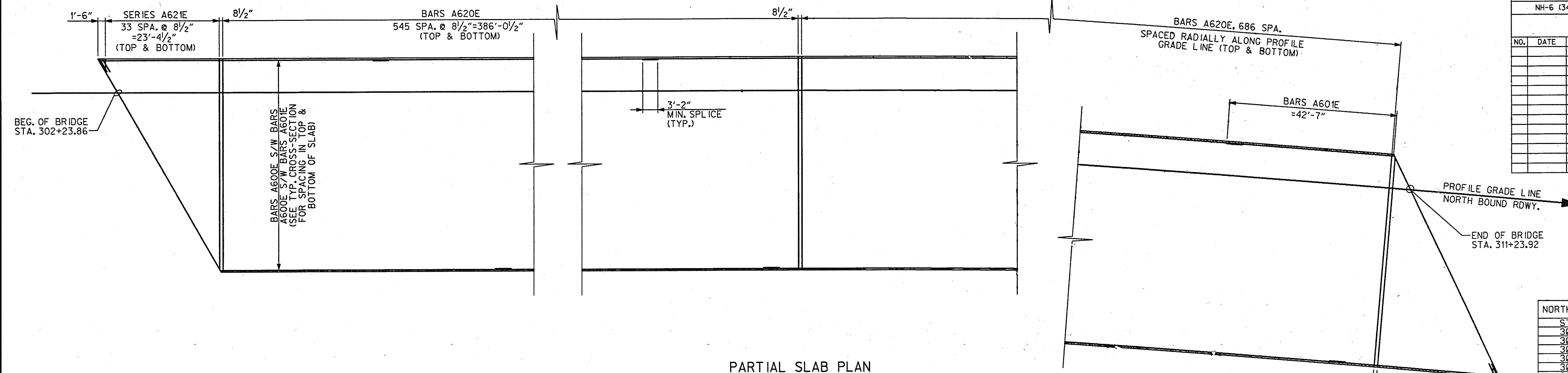
M-324-1



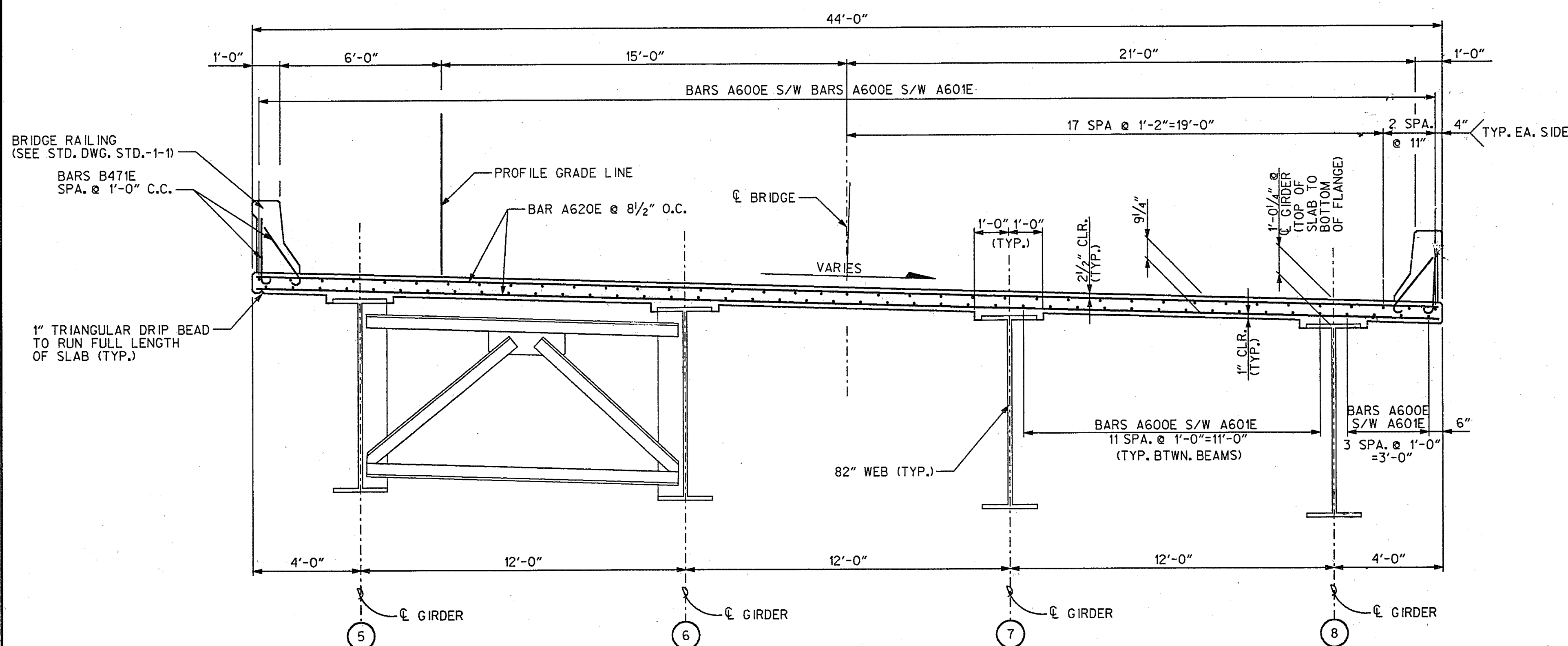
PROJECT NO.	YEAR	SHEET NO.
NH-6 (34)	1996	

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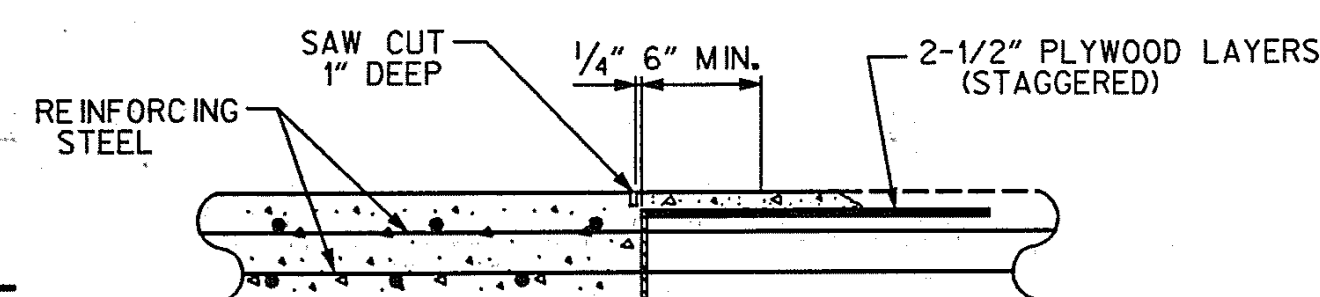
NORTHBOUND BRIDGE DECK DRAIN		
STATION	LOCATION	TYPE
303+50	RT.	CURB
304+50	RT.	CURB
305+00	RT.	CURB
305+50	RT.	CURB
306+00	RT.	CURB
306+50	RT.	CURB
307+00	RT.	CURB
307+50	RT.	CURB
308+00	RT.	CURB
308+25	RT.	CURB
308+50	RT.	CURB
308+75	RT.	CURB
309+00	RT.	GRATE
309+25	RT.	CIB
309+50	RT.	CURB
310+00	RT.	CURB
310+50	RT.	CURB
311+00	RT.	CURB



PARTIAL SLAB PLAN



CROSS SECTION



### SLAB CONSTRUCTION JOINT DETAIL

NOTE: THE SLAB SHALL NOT BE POURED UNTIL ALL STRUCTURAL STEEL IS ERECTED AND ALL WELDING OR BOLTING IS COMPLETE. THE CONTRACTOR SHALL MAKE ADEQUATE PROVISIONS DURING PLACEMENT OF SLAB TO PREVENT THE EXTERIOR BEAM FROM TWISTING. NO PORTION OF THE CURB OR PARAPET SHALL BE POURED UNTIL THE ENTIRE SLAB IS IN PLACE, UNLESS REQUIRED BY STAGE CONSTRUCTION.

DECK CONCRETE POURING SEQUENCE:

SLAB CONSTRUCTION JOINTS MAY BE LOCATED AT THE CONTRACTOR'S OPTION SUBJECT TO THE FOLLOWING:

- 1) THE CONSTRUCTION JOINT MAY BE LOCATED CLOSER THAN 10 FEET OR FURTHER THAN 15 FEET FROM AN INTERIOR SUPPORT.
- 2) THE SLAB IN THE MIDDLE SECTION OF BOTH ADJACENT SPANS MUST BE POURED TO WITHIN AT LEAST 15 FEET OF THE SUPPORTS EITHER PRIOR TO OR CONCURRENTLY WITH THE SLAB OVER AN INTERIOR SUPPORT.

NOTE: NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE.

NOTE: OUTSIDE EDGE OF SLAB AND BRIDGE RAIL TO CONFORM TO HORIZONTAL CURVE.

NOTE: THE CONCRETE DECK SHALL NOT BE POURED UNTIL ALL STRUCTURAL STEEL IS ERECTED AND ALL WELDING AND/OR BOLTING COMPLETE.

NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED.

IDENTITY OF MAIN MATERIALS: SEE SECTION 602 OF THE STANDARD SPECIFICATIONS.

STRUCTURAL STEEL: SHALL CONFORM TO AASHTO M270 (ASTM A709) GRADE 50W UNLESS OTHERWISE NOTED. ALL STRUCTURAL STEEL FOR GIRDER FLANGES IN TENSION AND ALL WEBS SHALL MEET THE SUPPLEMENTAL REQUIREMENTS FOR LONGITUDINAL CHAIR V-NOTCH TESTS SPECIFIED IN AASHTO MATERIAL SPECIFICATIONS, ZONE 2 OF NON-FRACTURE CRITICAL CRITERIA SHALL APPLY.

APPROVAL OF MATERIALS: NO FABRICATION SHALL BE STARTED UNTIL THE MATERIALS INVOLVED HAVE BEEN APPROVED BY THE TENNESSEE DEPARTMENT OF TRANSPORTATION, DIVISION OF MATERIALS AND TESTS.

ADDITIONAL SHOP SPlice NOTE: SHOP SPlices NECESSARY DUE TO LENGTHS OR SIZE OF MATERIAL INVOLVED MAY BE ADDED BY THE FABRICATOR SUBJECT TO APPROVAL BY THE ENGINEER AND SHALL BE AT NO ADDITIONAL COST TO THE PROJECT.

WELDING: ANSI/AASHTO/AWS D1.5-88 BRIDGE WELDING CODE AND SECTION 602 OF THE STANDARD SPECIFICATIONS.

FIELD CONNECTIONS: SHALL BE  $\frac{7}{8}$ " DIA. HIGH TENSILE STRENGTH BOLTS ASTM A325 UNLESS OTHERWISE SHOWN. SEE AASHTO SPECIFICATIONS ART. 11.3.2 DIVISION II AND SECTION 602 OF THE STANDARD SPECIFICATIONS.

HEAT CURVING STEEL GIRDERS: HEAT CURVING WILL BE PERMITTED FOR GIRDER WITH RADIUS OF CURVATURE IN EXCESS OF THE MINIMUM REQUIREMENTS SPECIFIED IN AASHTO ART. 10.15.2 DIV I, PROVIDED THE PROCEDURE IS IN ACCORDANCE WITH ART. 11.4.12 DIV II, AND THE VERTICAL CAMBER IS ADJUSTED IN ACCORDANCE WITH ART. 10.15.3 DIV I.

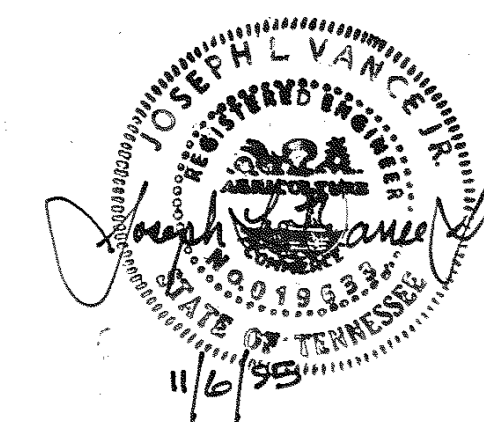
ADDITIONAL FIELD SPLICE NOTE: FIELD SPLICES NECESSARY DUE TO LENGTHS INVOLVED MAY BE ADDED BY THE FABRICATOR SUBJECT TO APPROVAL BY THE ENGINEER AND SHALL BE AT NO ADDITIONAL COST TO THE PROJECT.

SHOP ASSEMBLY: PROGRESSIVE SHOP ASSEMBLY WILL BE ALLOWED. SEE AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, ART. 11.5.3.1 DIVISION II.

ESTIMATED QUANTITIES	
CLASS "D" CONCRETE (BRIDGES) C.Y.	EPOXY COATED REINFORCING STEEL (BRIDGES) LB.
1.168	291.810

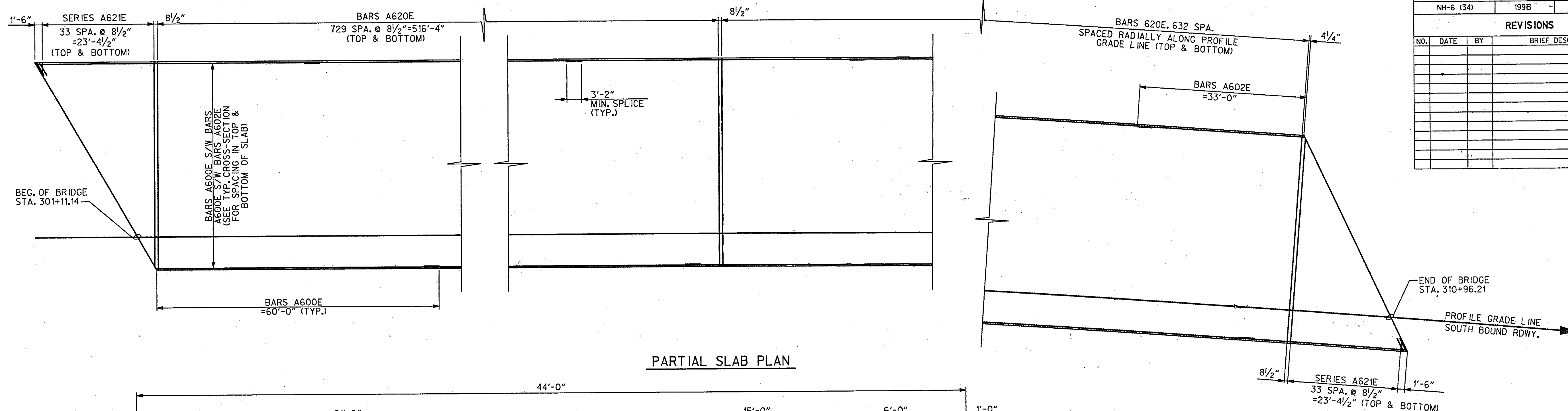
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE  
 BRIDGE NO. 1 NORTHBOUND  
 STATE ROUTE 6 OVER BIG BIGBY CREEK  
 AND TENNESSEE SOUTHERN RAILROAD  
 STATION 306+00  
 MAURY COUNTY  
 1995

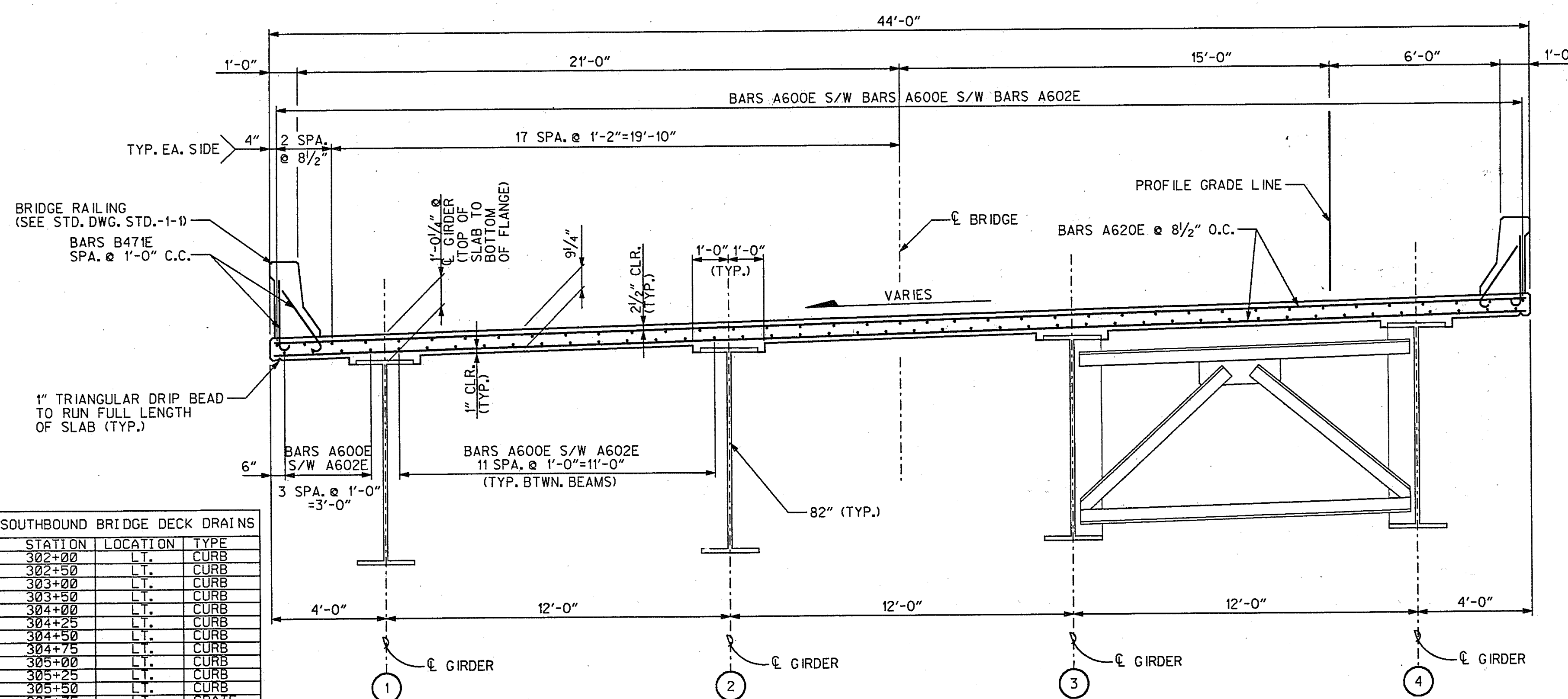

**GRESHAM, SMITH AND PARTNERS**  
 Birmingham • Jacksonville • Nashville

DESIGNED BY J. VANCE DATE 8/95  
DRAWN BY R. VERNON DATE 9/95  
SUPERVISED BY J.M. BARRY DATE 10/95  
CHECKED BY J. VANCE DATE 10/95

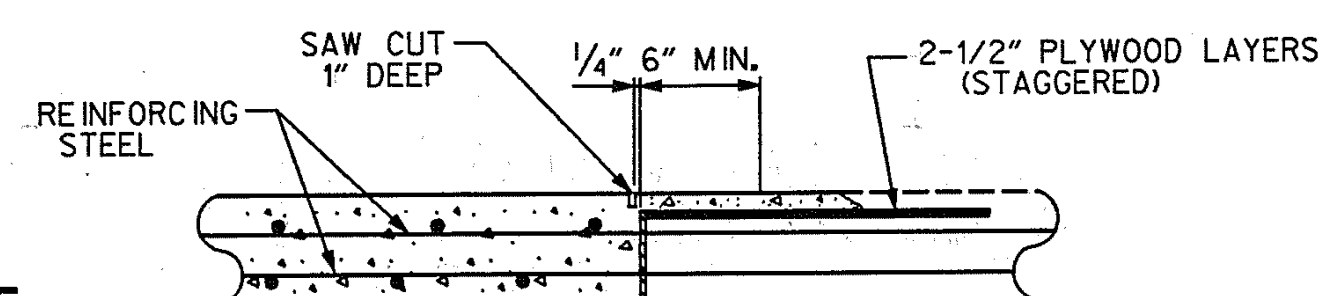




PARTIAL SLAB PLAN



CROSS SECTION



### SLAB CONSTRUCTION JOINT DETAIL

NOTE:  
THE SLAB SHALL NOT BE POURED UNTIL ALL STRUCTURAL STEEL IS ERECTED AND ALL WELDING OR BOLTING IS COMPLETE. THE CONTRACTOR SHALL MAKE ADEQUATE PROVISIONS DURING PLACEMENT OF SLAB TO PREVENT THE EXTERIOR BEAM FROM TWISTING. NO PORTION OF THE CURB OR PARAPET SHALL BE POURED UNTIL THE ENTIRE SLAB IS IN PLACE, UNLESS REQUIRED BY STAGE CONSTRUCTION.

DECK CONCRETE POURING SEQUENCE:

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NOTE: NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE.

NOTE: OUTSIDE EDGE OF SLAB AND BRIDGE RAIL TO CONFORM TO HORIZONTAL CURVE.

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WELDING: ANSI/AASHTO/AWS D1.5-88 BRIDGE WELDING CODE AND SECTION 602 OF THE STANDARD SPECIFICATIONS.

FIELD CONNECTIONS: SHALL BE 7/8" DIA. HIGH TENSILE STRENGTH BOLTS ASTM A325  
UNLESS OTHERWISE SHOWN, SEE AASHTO SPECIFICATIONS ART. 11.3.2 DIVISION II AND  
SECTION 602 OF THE STANDARD SPECIFICATIONS.

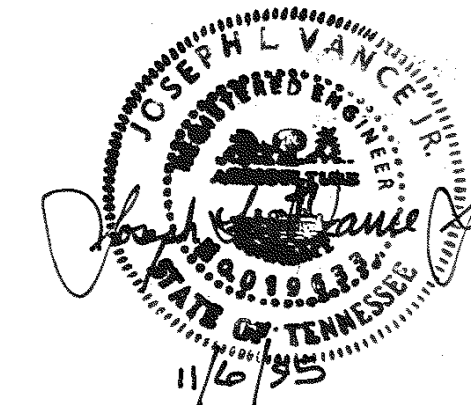
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ESTIMATED QUANTITIES

CLASS "D" CONCRETE (BRIDGES) C.Y.	EPOXY COATED REINFORCING STEEL (BRIDGES) LB.
1,281	321,787



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE  
BRIDGE NO. 1 SOUTHBOUND  
STATE ROUTE 6 OVER BIG BIGBY CREEK  
AND TENNESSEE SOUTHERN RAILROAD  
STATION 306+00  
MAURY COUNTY  
1995

DESIGNED BY	<u>J. VANCE</u>	DATE	<u>8/95</u>
DRAWN BY	<u>R. VERNON</u>	DATE	<u>9/95</u>
SUPERVISED BY	<u>J.M.BARRY</u>	DATE	<u>10/95</u>
CHECKED BY	<u>J. VANCE</u>	DATE	<u>10/95</u>





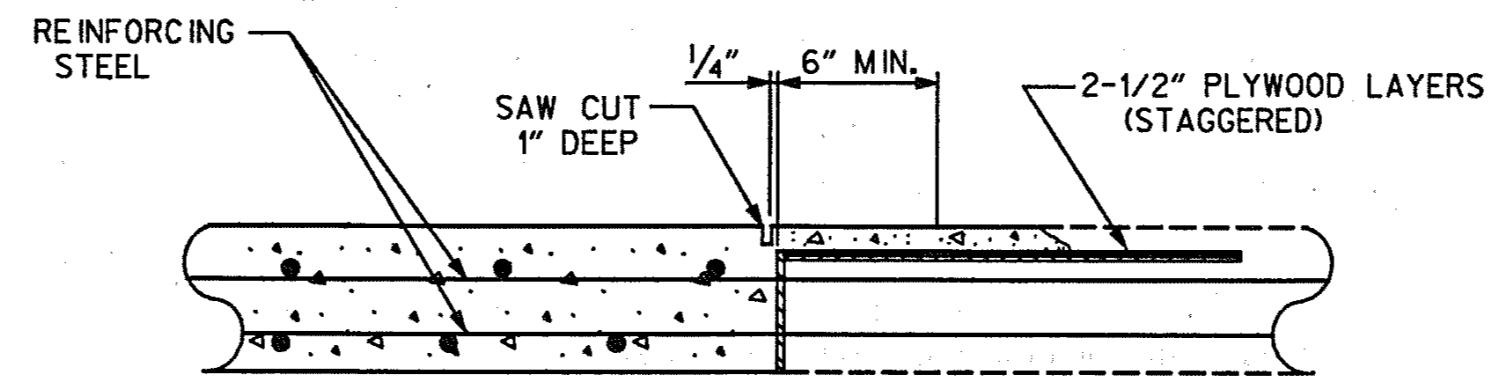
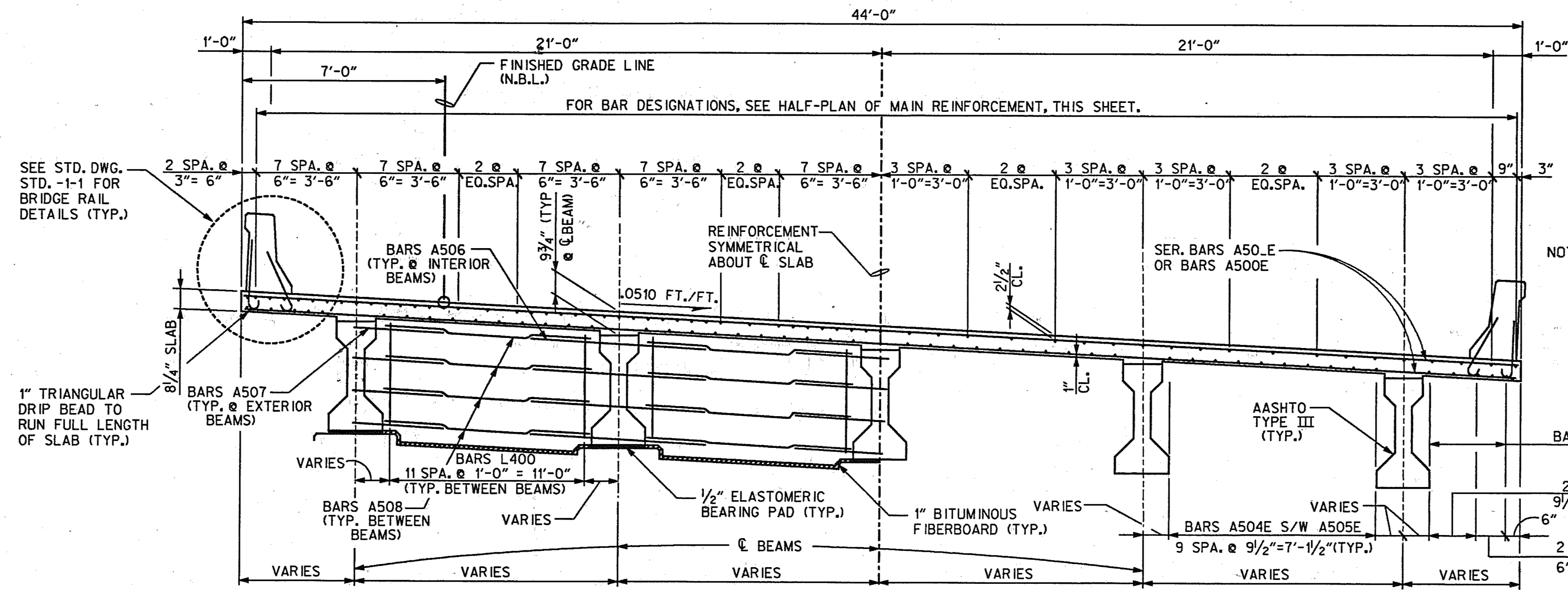


CONST. NO. 60002-3269-14

PROJECT NO.	YEAR	SHEET NO.
NH-6 (34)	1996	

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION



SLAB CONSTRUCTION JOINT DETAIL

NOTE: DECK CONCRETE POURING SEQUENCE: SLAB CONSTRUCTION JOINTS MAY BE LOCATED AT THE CONTRACTOR'S OPTION SUBJECT TO THE FOLLOWING:  
 1) NO CONSTRUCTION JOINT MAY BE LOCATED CLOSER THAN 10 FEET OR FURTHER THAN 15 FEET FROM AN INTERIOR SUPPORT.  
 2) THE SLAB IN THE MIDDLE SECTION OF BOTH ADJACENT SPANS MUST BE POURED TO WITHIN AT LEAST 15 FEET OF THE SUPPORTS EITHER PRIOR TO OR CONCURRENTLY WITH THE SLAB OVER AN INTERIOR SUPPORT.  
 ALL SLAB CONSTRUCTION JOINTS SHALL BE IN ACCORDANCE WITH THE SLAB CONSTRUCTION JOINT DETAIL SHOWN ABOVE.

NOTES

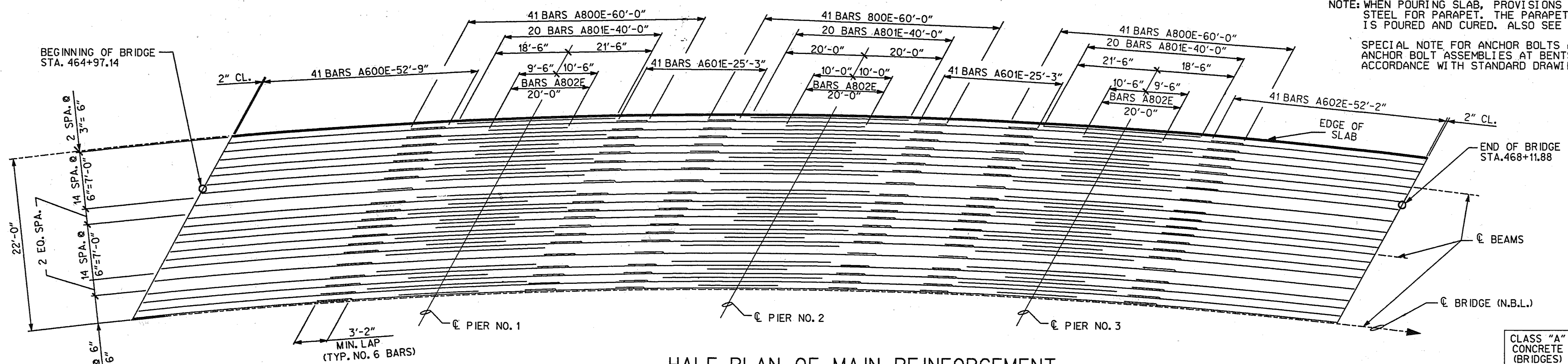
NOTE: THE CONTRACTOR IS SOLELY RESPONSIBLE FOR SUPPORTING THE BEAMS TO PREVENT DAMAGE DUE TO TWISTING OR OVERTURNING DURING ALL PHASES OF CONSTRUCTION. IT IS STRONGLY RECOMMENDED THAT THE TEMPORARY ERECTION DIAPHRAGMS BE INSTALLED AND THE PERMANENT INTERMEDIATE DIAPHRAGMS BE POURED AND CURED PRIOR TO PLACING ANY LOADS ON THE GIRDERS HOWEVER, TEMPORARY ERECTION DIAPHRAGMS AND PERMANENT INTERMEDIATE DIAPHRAGMS MUST BE IN PLACE IN THE SPAN AT THE TIME THE SLAB IS POURED IN SAID SPAN.  
 NOTE: SUPPORT DIAPHRAGMS AT PIERS SHALL BE POURED CONCURRENTLY WITH THE DECK SLAB AND INCLUDED IN THE QUANTITY FOR ITEM 604-03.09.  
 NOTE: OUTSIDE EDGE OF SLAB AND BRIDGE RAIL TO CONFORM TO HORIZONTAL CURVE.  
 NOTE: NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE.  
 NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED. ALSO SEE DRAWING STD. -1-1.  
 SPECIAL NOTE FOR ANCHOR BOLTS AT BENTS: ANCHOR BOLT ASSEMBLIES AT BENTS SHALL BE IN ACCORDANCE WITH STANDARD DRAWING STD-6-1.

TYPICAL HALF SECTION

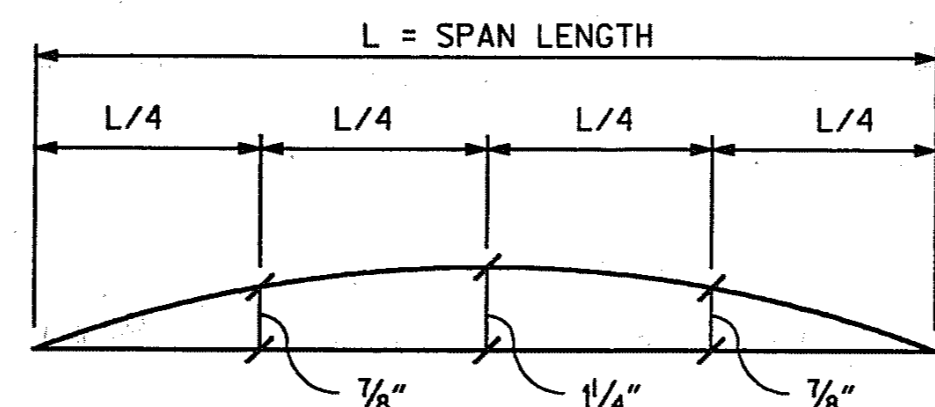
• SUPPORTS

TYPICAL HALF SECTION

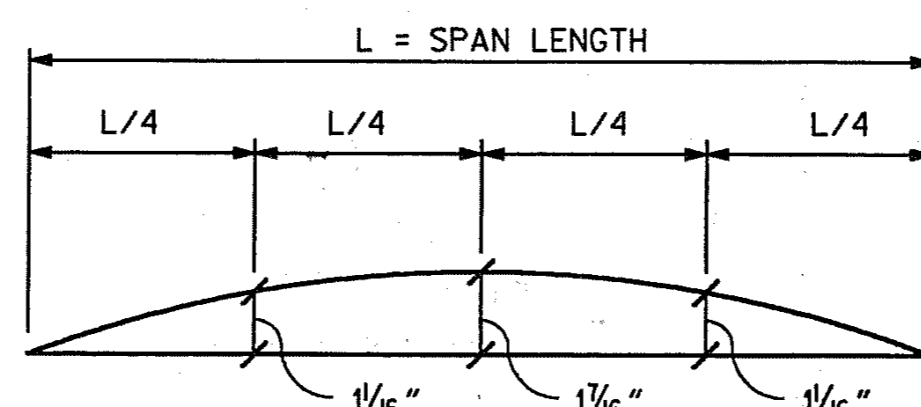
• MIDSPAN



HALF-PLAN OF MAIN REINFORCEMENT



SPANS 1 AND 4



SPANS 2 AND 3

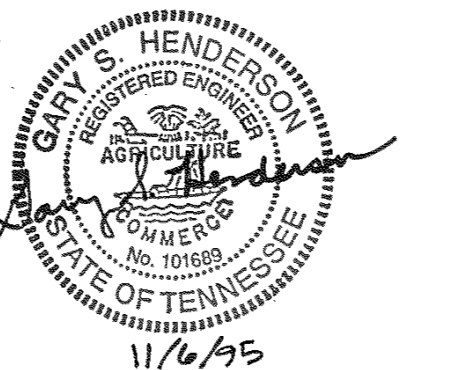
DEAD LOAD CORRECTION CURVE

ESTIMATED QUANTITIES

CLASS "A" CONCRETE (BRIDGES) C.Y.	CLASS "D" CONCRETE (BRIDGE DECKS) C.Y.	EPOXY COATED REINFORCING STEEL (BRIDGES) LB.	STEEL BAR REINFORCEMENT (BRIDGES) LB.
27	357	111,738	2,793

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

BRIDGE NO.2  
SUPERSTRUCTURE (NBL)  
STATE ROUTE 6 OVER BIG BIGBY CREEK  
STATION 466+79.00  
MAURY COUNTY  
1995



GRESHAM, SMITH AND PARTNERS  
Birmingham · Jacksonville · Nashville

DESIGNED BY G. HENDERSON DATE 11/94  
 DRAWN BY B. FRALIX DATE 09/95  
 SUPERVISED BY J.M. BARRY DATE 09/95  
 CHECKED BY G. HENDERSON DATE 09/95

NOTE: DEAD LOAD CORRECTION CURVE: THIS CURVE IS FOR DEAD LOAD SLAB AND ALL DEAD LOADS THAT ARE APPLIED AFTER SLAB IS IN PLACE.  
 IF PRESTRESSED DECK PANELS ARE USED AND THE BEAMS ARE PROFILED AFTER PANELS ARE IN PLACE, REDUCE THE DEAD LOAD CORRECTION VALUES SHOWN BY 25%.

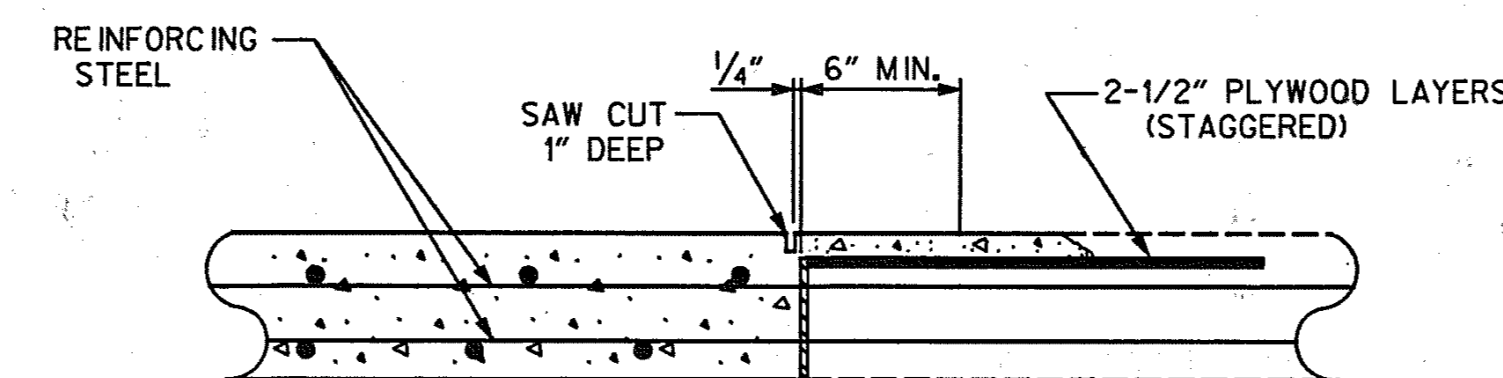


## NOTES

SPECIAL NOTE FOR ANCHOR BOLTS AT BENTS:  
ANCHOR BOLT ASSEMBLIES AT BENTS SHALL BE  
IN ACCORDANCE WITH STANDARD DRAWING STD-6-1.



BRIDGE NO.2  
SUPERSTRUCTURE (SBL)  
STATE ROUTE 6 OVER BIG BIGBY CREEK  
STATION 466+79.00  
MAURY COUNTY  
1995



NOTE: DECK CONCRETE POURING SEQUENCE: SLAB CONSTRUCTION JOINTS MAY BE LOCATED AT THE CONTRACTOR'S OPTION SUBJECT TO THE FOLLOWING:

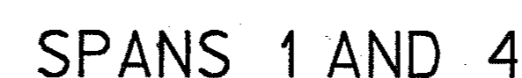
- 1) NO CONSTRUCTION JOINT MAY BE LOCATED CLOSER THAN 10 FEET OR FURTHER THAN 15 FEET FROM AN INTERIOR SUPPORT.
- 2) THE SLAB IN THE MIDDLE SECTION OF BOTH ADJACENT SPANS MUST BE POURED TO WITHIN AT LEAST 15 FEET OF THE SUPPORTS EITHER PRIOR TO OR CONCURRENTLY WITH THE SLAB OVER AN INTERIOR SUPPORT.

ALL SLAB CONSTRUCTION JOINTS SHALL BE IN ACCORDANCE WITH THE SLAB CONSTRUCTION JOINT DETAIL SHOWN ABOVE.

### SLAB CONSTRUCTION JOINT DETAIL

TYPICAL HALF SECTION

TYPICAL HALF SECTION



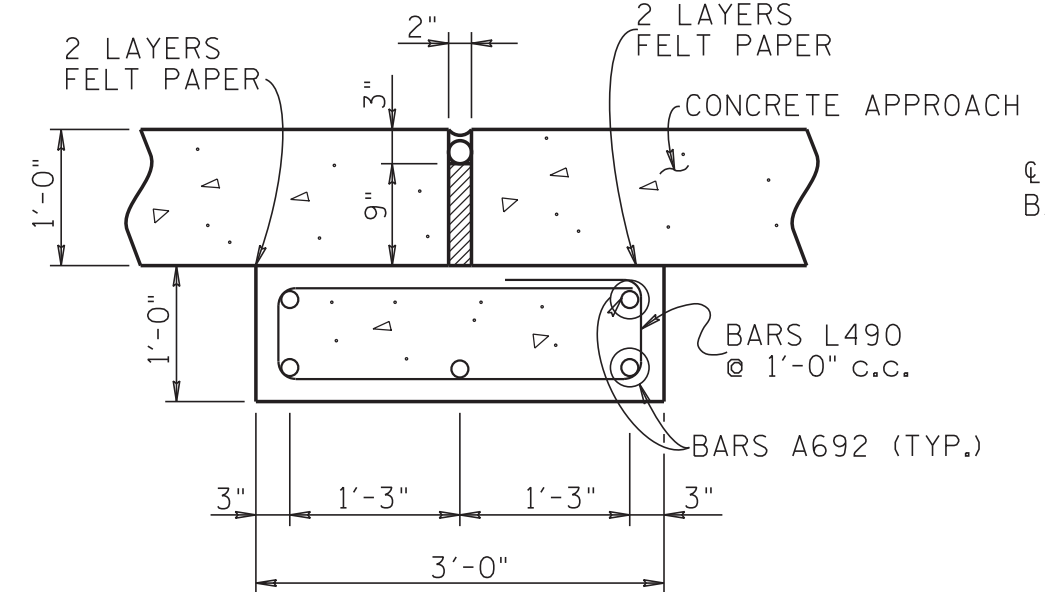
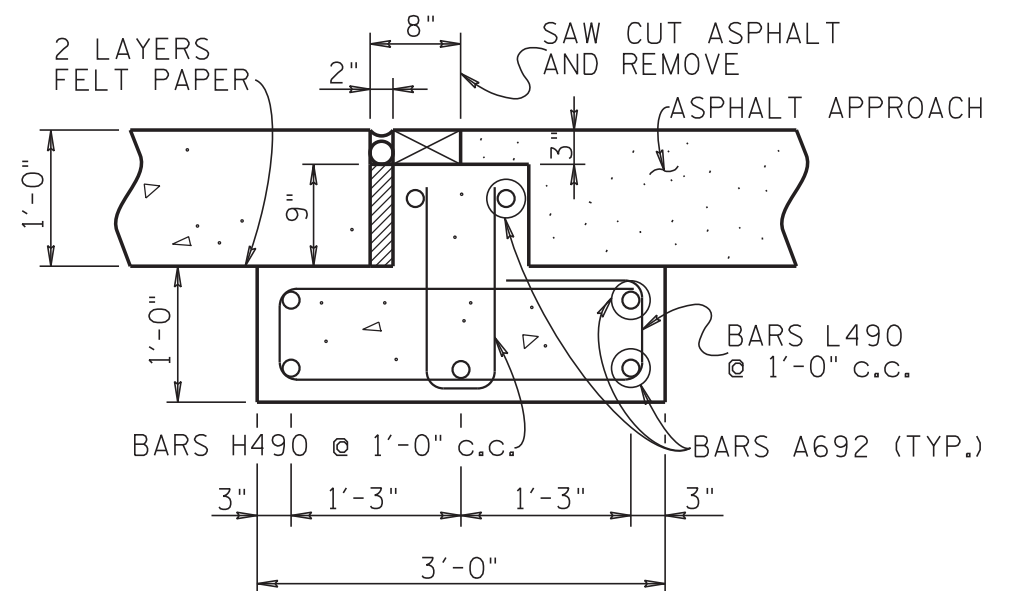
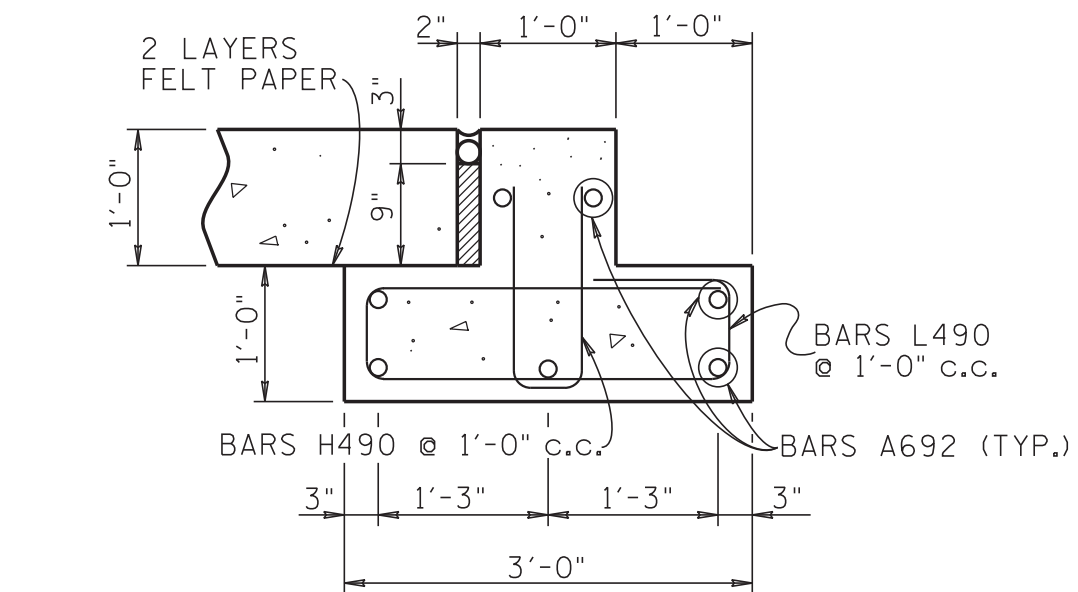
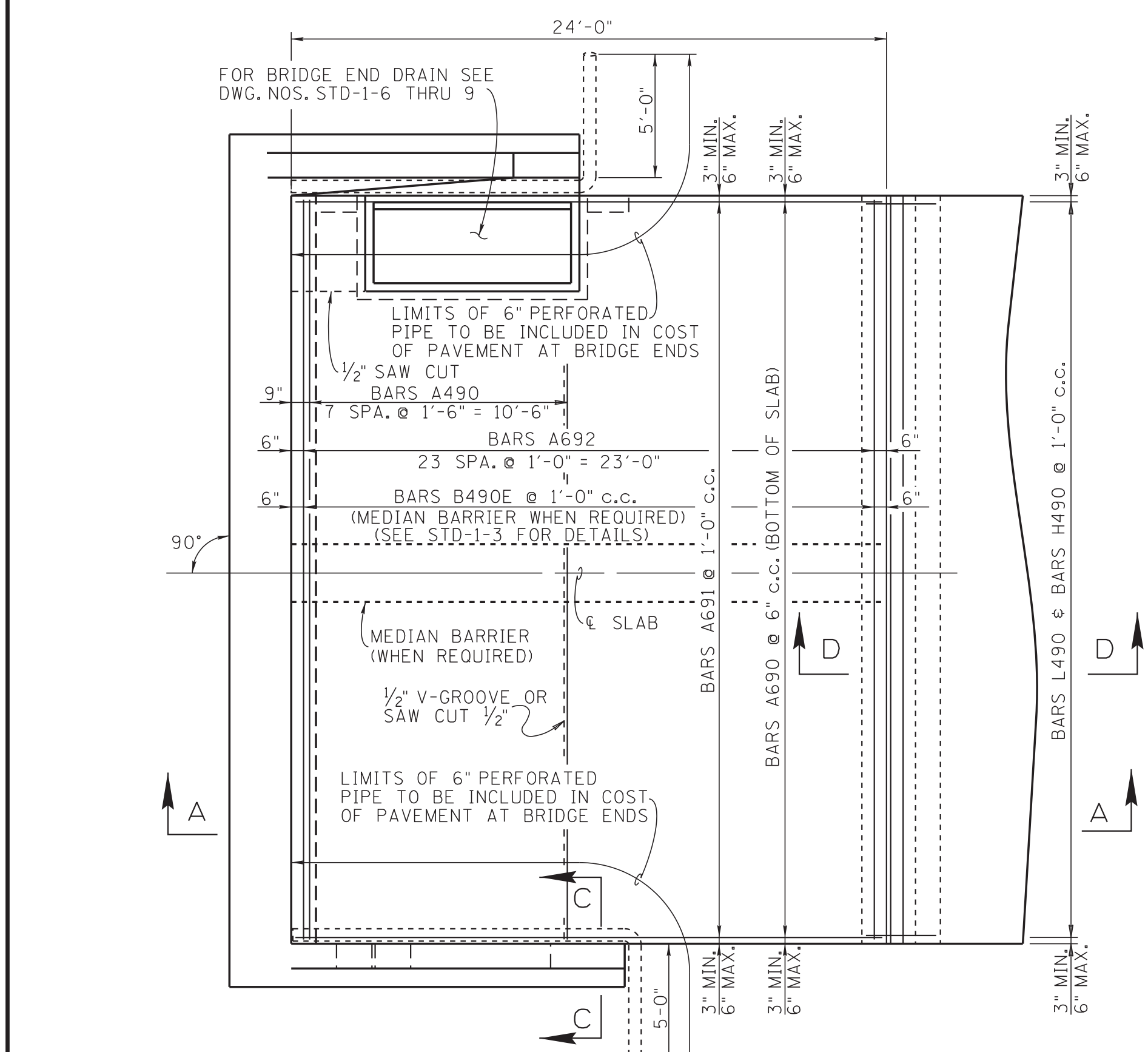
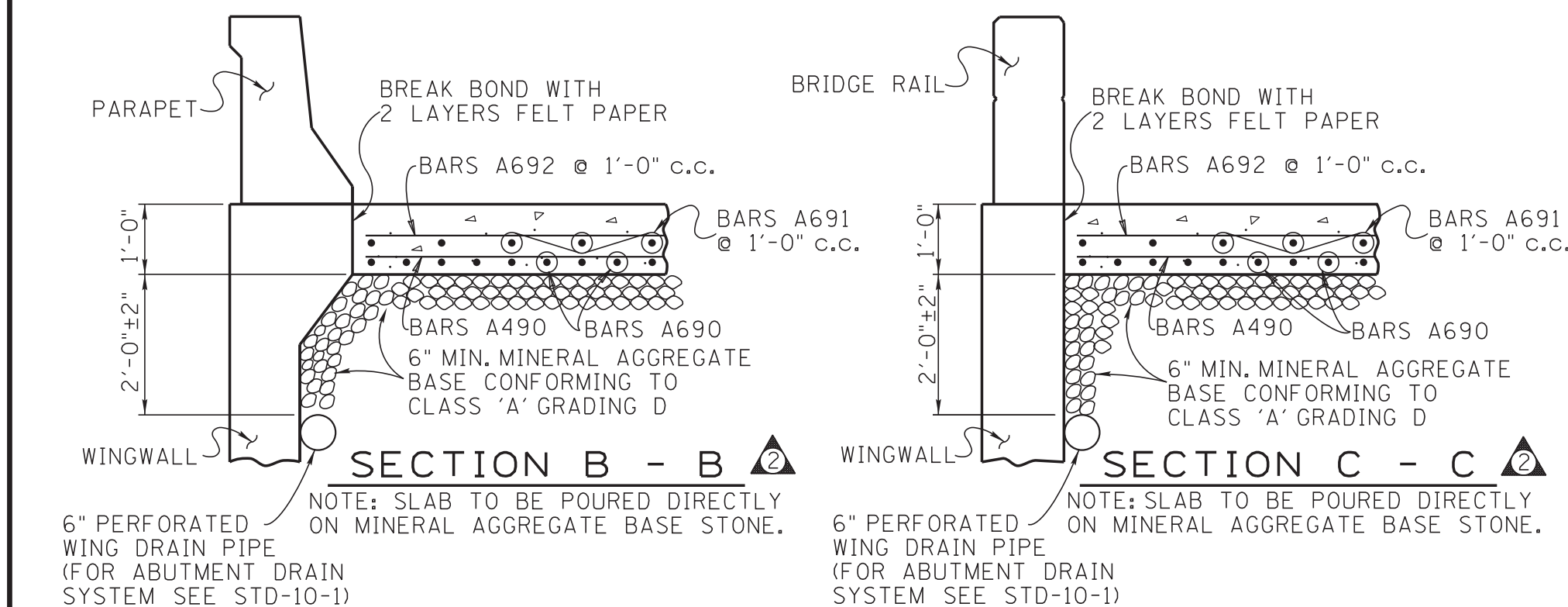
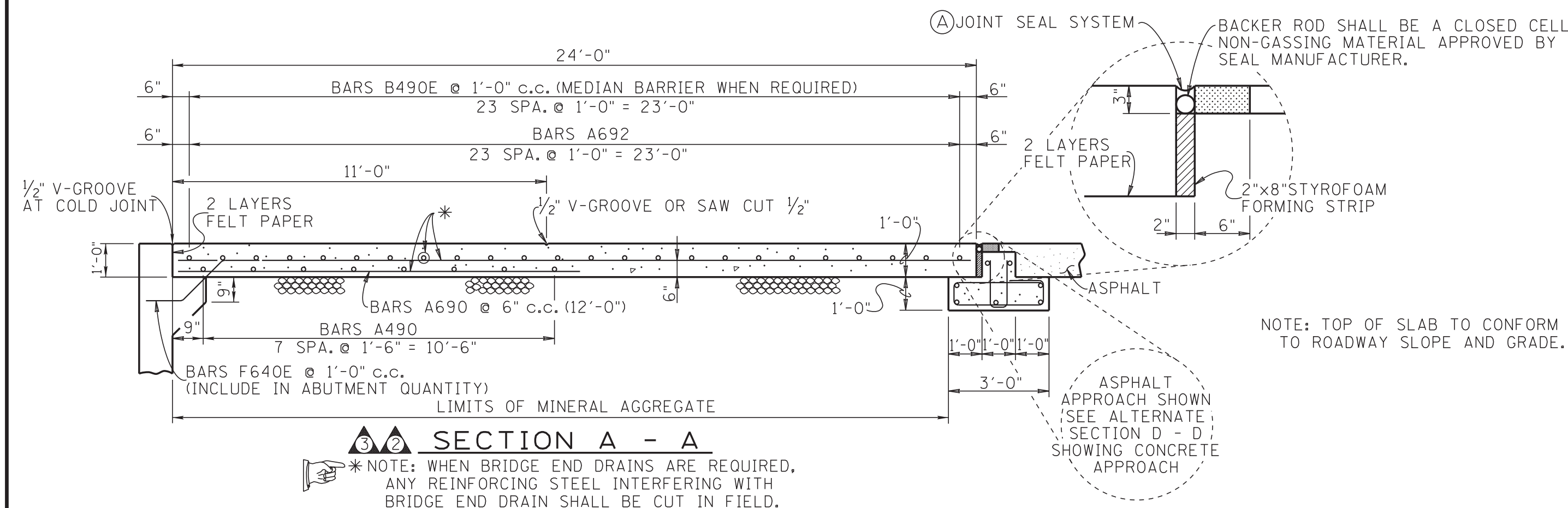
DEAD LOAD CORRECTION CURVE

ESTIMATED QUANTITIES			
CLASS "A" CONCRETE (BRIDGES) C.Y.	CLASS "D" CONCRETE (BRIDGE DECKS) C.Y.	STEEL BAR REINFORCEMENT (BRIDGES) LB.	EPOXY COATED REINFORCING STEEL (BRIDGES) LB.
26	347	109,081	2,768

NOTE: DEAD LOAD CORRECTION CURVE: THIS CURVE IS FOR DEAD LOAD SLAB AND ALL DEAD LOADS THAT ARE APPLIED AFTER SLAB IS IN PLACE.

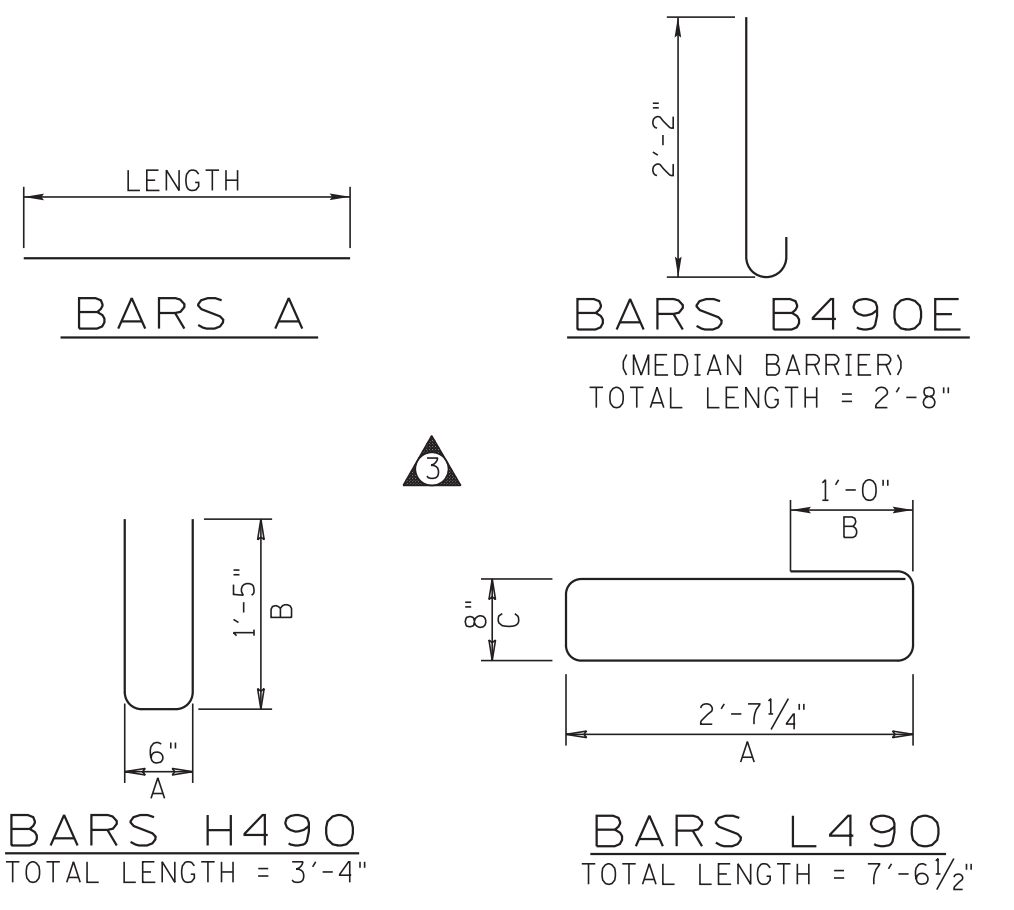
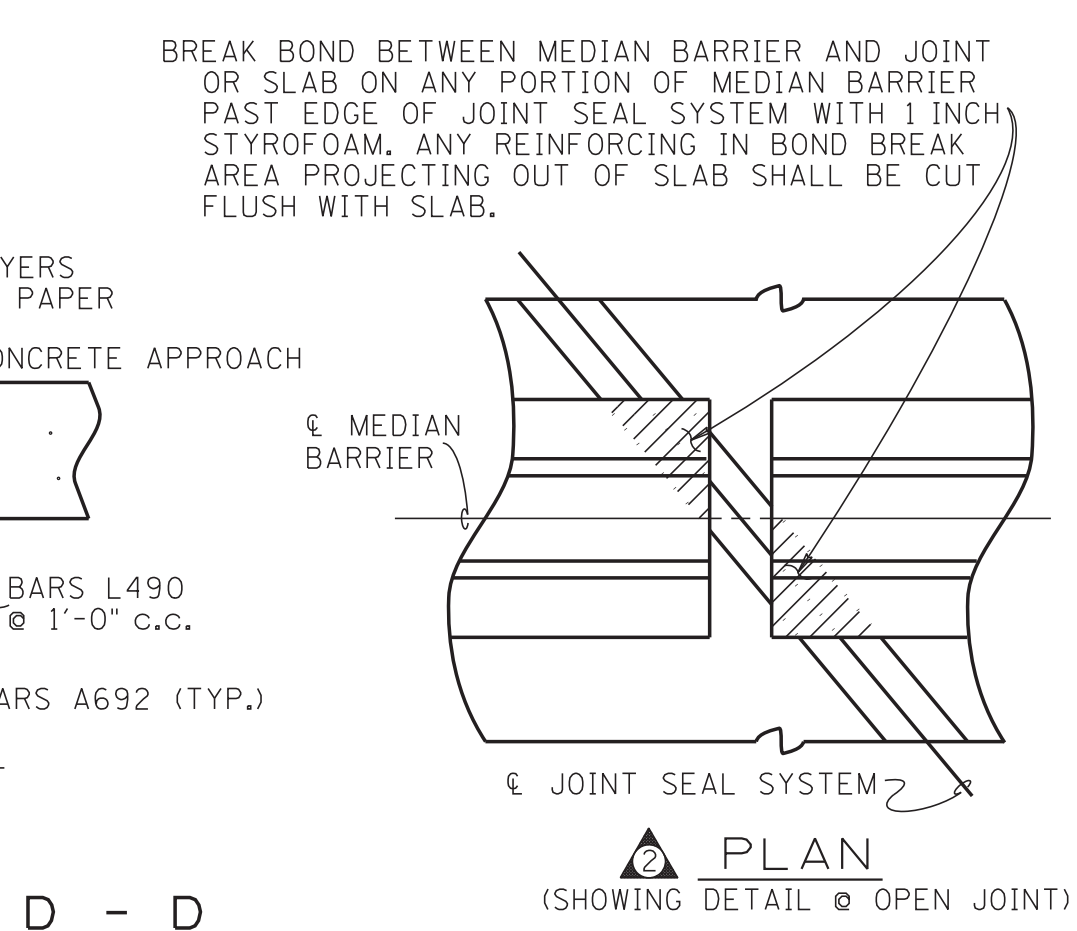
IF PRESTRESSED DECK PANELS ARE USED AND THE BEAMS ARE PROFILED AFTER  
PANELS ARE IN PLACE, REDUCE THE DEAD LOAD CORRECTION VALUES SHOWN  
BY 25%.





BILL OF STEEL								
BARS	LOCATION	SIZE	NO. REQ'D	BENDING DIMENSIONS				LENGTH
				A	B	C	D	
B490E	MEDIAN (WHEN REQ'D.)	4	48					2'-8"
A490	SLAB	4	8					△
A690	SLAB	6	△					12'-0"
A691	SLAB	6	△					23'-8"
A692	SLAB ☆	6	31					△
H490	FOOTING ☆	4	△	6"	1'-4"			3'-4"
L490	FOOTING	4	△	2'-7 1/4"	1'-0"	8"		7'-6 1/2"

△ THESE NUMBERS VARY DEPENDING UPON ROADWAY WIDTH.

[illegible]

- ## NOTES
1. QUANTITIES FOR CLASS 'A' CONCRETE, REGULAR AND EPOXY COATED REINFORCING STEEL (WHEN REQUIRED FOR MEDIAN BARRIER), BACKER ROD, JOINT SEALER, ELASTOMERIC CONCRETE, STYROFOAM, GRATE AND MISCELLANEOUS MATERIALS FOR BRIDGE END DRAIN, WHEN REQUIRED, ARE TO BE INCLUDED IN PAVEMENT AT BRIDGE ENDS, S.Y. FOR BAR BENDING DIMENSIONS SEE THIS SHEET AND BILL OF STEEL FOR BRIDGE END DRAIN ON DRAWING NO. STD-1-6.
  2. COST OF MINERAL AGGREGATE CLASS A GRADING D BASE QUANTITY SHALL BE INCLUDED IN COST OF PAVEMENT AT BRIDGE ENDS. CLASS B GRADING C OR D MAY ALSO BE USED.

- ## GENERAL NOTES
- CONCRETE: TO BE CLASS 'A' ( $f'_c = 3,000$  psi)
- REINFORCING STEEL: SHALL BE ASTM A615 GRADE 60 UNLESS NOTED OTHERWISE. SEE SECTION 604 AND 907 OF THE STANDARD SPECIFICATIONS.
- SPECIFICATIONS: STANDARD ROAD AND BRIDGE SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION (CURRENT EDITION).
- NOTE: THE APPROACH SLAB SHALL NOT BE POURED UNTIL THE ADJACENT END SPAN DECK SLAB IS IN PLACE AND ACCEPTED BY THE ENGINEER.
- NOTE: THE APPROACH SLAB CONTROL ELEVATIONS SHALL BE ADJUSTED, (IF REQUIRED), SO AS TO MATCH THE IN PLACE DECK SLAB IN BOTH TRANSVERSE AND LONGITUDINAL DIRECTIONS.

- 2 (A) JOINT SEAL SYSYTEM:  
3 THE EXPANSION JOINT SYSTEM USED SHALL BE ON THE TDOT  
4 APPROVED QUALIFIED LISTS FOR ACCEPTABLE PRODUCTS.  
THE JOINT SYSTEM SHALL BE INSTALLED UNDER THE DIRECT  
SUPERVISION OF AN AUTHORIZED TECHNICIAN PROVIDED  
BY THE EXPANSION JOINT SUPPLIER.

$\frac{M}{R}$  MINOR REVISION - FHWA  
APPROVAL NOT REQUIRED

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

REINFORCED CONCRETE  
PAVEMENT AT BRIDGE ENDS  
1995

DESIGNED BY C.M. HILES DATE \_\_\_\_\_  
 DRAWN BY KIM FRANKENFIELD DATE 4-95  
 SUPERVISED BY C.M. HILES DATE 4-95  
 CHECKED BY \_\_\_\_\_ DATE \_\_\_\_\_



PLAN  
(90° SKEW)

△<sub>3</sub>△<sub>2</sub> PLAN  
(VARIABLE SKEW)

CORRECT Edward P. Wasserman  
ENGINEER OF STRUCTURES